



TOWN OF AVON

BICYCLE & PEDESTRIAN MASTER PLAN

FEBRUARY 2024



Acknowledgements

This project would not have been possible without a committed **Advisory Team at the Town of Avon** who provided guidance and support:

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Bruce Williams, Director of Public Works

The opinions, ideas, energy and enthusiasm of Bike Walk Avon, CT, the Green Valley Volunteers, and the many residents who participated in this project's three public meetings was also indispensable. The "Slow Down Avon" movement dovetailed nicely with this project to amplify the message of road safety for all road users.

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Executive Summary

This report documents the process undertaken to arrive at actionable recommendations to create safer, more connected roadways for Avon’s bicyclists, pedestrians, and drivers. The value of a study such as this is apparent when we assess what change it has already catalyzed in Avon since this study’s inception in January of 2023. The Town of Avon has been **awarded bronze-level Bicycle Friendly Community status; passed a Complete Streets Resolution; received preliminary approval for a \$4 million grant to improve Country Club Road and install a pedestrian facility from the library east to Old Farms Road; applied for a grant to install bus shelters** at five locations on the south side of Route 44; and **made the decision that the vehicle travel lane may be as narrow as 10 feet** on low-traffic volume *Collector* roads or roads classified as *Local* – which can provide more space for pedestrians and cyclists along the road edge, and calm traffic.

The recommendations that make up the Implementation Plan grew out of an analysis of the **Existing Conditions (Chapter 1.4)** – including sidewalk, bus stop, trailhead, crosswalk, bike parking, and crash locations, as well as the determination of important destinations and how they could be better connected by creating walksheds (the area around a point of interest that is reachable on foot for the average person) around them.

The recommendations were also informed by the results of this project’s **Outreach (Chapter 2)** – eliciting input from public meetings held in April, June, and September of 2023; feedback on 4 Monthly Mobility Questions; regular meetings with the town’s Advisory Group; **Concept Development (Chapter 3)** based on assessments of the opportunities and constraints on the town’s major roadways; and on planning and engineering best practice.

Recommendations (Chapter 4) fall into the categories of policy and education/program recommendations – such as changes to zoning language and painting a speed limit reminder on the Farmington Canal Heritage Trail entry points – as well as bicycle and pedestrian infrastructure recommendations. These infrastructure recommendations are listed alphabetically by street and shown graphically in **Appendix C**. The plan for implementing and prioritizing these recommendations is described in the **Implementation Plan (Chapter 5)**. Specific focus was given to Route 44 – which has sidewalk in some segments, all of the CTtransit bus stops, and much of the town’s retail and commercial business activity. Route 44 is also considered to be the town’s High Crash Corridor, from Nod Road west to the Simsbury Town line in the Capitol Region Council of Government (CROG)’s 2023 *Regional Transportation Safety Plan*. Another focus area was Country Club Road – an important east/west connection through town that provides access to the library, Farmington Canal Heritage Trail, and extensive trail system at the Fisher Meadows Recreation Area. Assuming the grant application funds are allocated, an important link will be created for pedestrians and cyclists through Avon. These infrastructure improvements would be augmented by the two-phase Old Farms Road Project. Phase I (North/South) will construct a paved multi-use trail beginning on Scoville Road at the Farmington Canal Heritage Trail, continuing to the intersection with Old Farms Road, and proceeding south along Old Farms Road to the intersection with Thompson Road. Phase II (East/West) would create a 1.25 mile loop trail from the Thompson Road intersection, west along the north side of Thompson Road back to the Farmington Canal Heritage Trail and would also include a trail segment heading east along Old Farms Road where it would meet the Fisher Meadows Recreation area. Another area to consider lending focus is Lovely Street – given the support and enthusiasm brought to that specific roadway by the “Green Valley Volunteers.” The addition of buffered bike lanes or a two-way multi-use path could be added, the latter accommodating both pedestrians and cyclists.

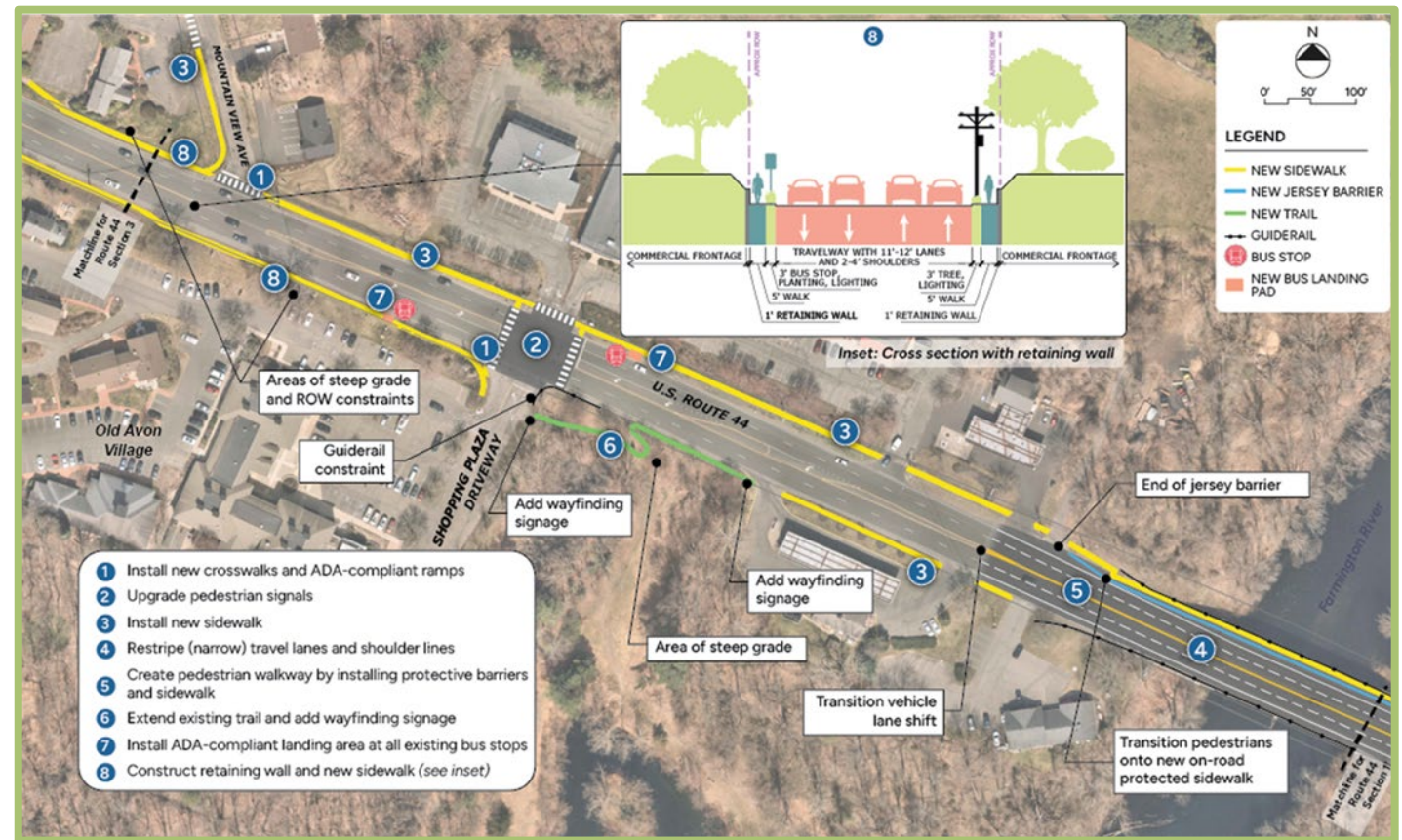


Figure 1: Route 44 Pedestrian Improvements near Farmington River Bridge

Note: Full-sized map in Appendix D

This study has resulted in over a hundred recommendations throughout the Town of Avon, and an Implementation Matrix has been developed to help guide decision-making; this Matrix is included in **Appendix D**. Each individual improvement (as described in **Chapter 4** and shown graphically in **Appendix C**) is listed and sorted alphabetically by street. Additional elements have been added to each recommendation, including a cost estimate, timeframe, and identification of any potential connectivity or safety concerns keyed to each recommendation. The Matrix has also been provided to the Town of Avon staff in spreadsheet form so all recommendations can be sorted or filtered by criteria as needed. There are also several public comments in **Appendix B** that may guide the town’s decisions.



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Acronyms and Abbreviations

AADT	Average Annual Daily Traffic
AASHTO	American Association of State Highway and Transportation Officials
ADA	Americans with Disabilities Act
CRCOG	Capitol Region Council of Governments
CTDOT	Connecticut Department of Transportation
FHWA	Federal Highway Administration
LOTICIP	Local Transportation Capital Improvement Program
LPI	Lead Pedestrian Interval
MUTCD	Manual on Uniform Traffic Control Devices
mph	miles per hour
POCD	Plan of Conservation and Development
ROW	Right of Way
RRFB	Rectangular Rapid Flashing Beacon
SLR	SLR International Corporation
STEAP	Small Town Economic Assistance Program



1.0 Project Background

Avon’s Bicycle and Pedestrian Master Plan project is an assessment of the Town of Avon’s existing bicycle and pedestrian facilities to determine how they can better connect to each other and be made safer for those who exercise or recreate here, and those who use their feet or human-powered wheels for transportation.

1.1 Support and Collaboration

The Town of Avon’s Planning and Zoning Commission endorses Complete Streets principles; and local non-profit Bike Walk Avon, CT lends energy, ideas, and volunteer effort to programs and initiatives that advance bicycle and pedestrian improvements. This support and active engagement has enabled passage of a Complete Streets Resolution by the Town Council on June 1, 2023, providing a policy framework that promotes bicycle and pedestrian amenities and improvements:

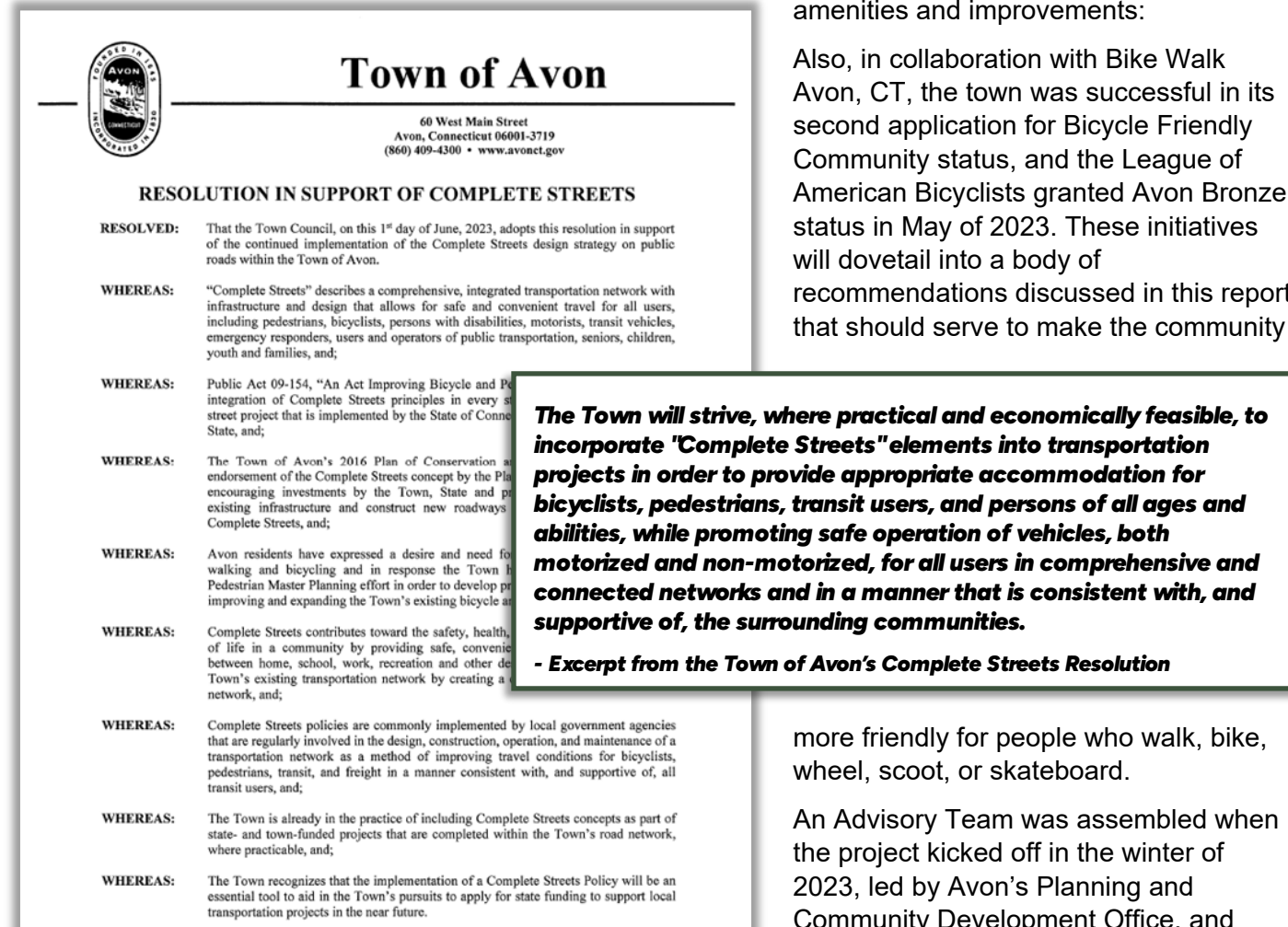


Figure 2: Complete Streets Resolution (2023)

Police Department. The Project and Advisory Team started looking at maps and studies, meeting with Bike

Walk Avon, and posing Monthly Mobility Questions through the project’s StoryMap website and social media posts, as well as on map boards at the library and at the Senior Center.

1.2 Project Vision

Before asking the first Monthly Mobility Question, the project team requested feedback on the following draft Vision Statement:

The Town of Avon, Connecticut’s transportation network will provide improved mobility, connectivity with existing trails, public health, and recreational opportunities. It will enhance sustainability by encouraging nonmotorized or public transportation options as an alternative to motor vehicle travel.

The statement was modified using provided feedback and reads as follows:

To facilitate public health, recreational opportunities, and better air quality, the Town of Avon strives to improve access and safety for all who use our road and trail networks including bicyclists, pedestrians, and motor vehicle operators. To achieve this vision, the town may invest in infrastructure such as sidewalks, bicycle facilities, traffic calming measures, and programming such as road safety education for all users.

1.3 Historical Context

Avon residents’ perspective on sidewalks changed over the last five decades as it has grown from a rural community to a low-density suburban community. Generally, people are more aware of the connection between exercise and human health today, and there is an increased interest in walking and bicycling as transportation modes. Prospective homebuyers in a community like Avon look for bicycle- and pedestrian-friendly amenities, like sidewalks and bike lanes that facilitate a healthy, active lifestyle.

In 1982, a Sidewalk Study Committee appointed by the Board of Selectmen delivered a report that ranked five roads as priorities for sidewalk construction/reconstruction. Country Club Road ranked first on the list. This priority was reaffirmed in the Town of Avon’s 2016 Plan of Conservation and Development (POCD) which recommended sidewalks on Country Club Road as a priority. This Bicycle and Pedestrian Master Plan emphasizes the creation of pedestrian connections between residential neighborhoods, and the town’s centers of activity such as commercial districts, municipal and school facilities, and recreation areas including the 4.8 miles of the Farmington Canal Heritage Trail that runs north-south through town.

Note: This text adapted from the Local Transportation Capital Improvement Program (LOTICIP) application for Country Club Road improvements.

1.3.1 Previous Studies

Route 44 Corridor Study (2000)

The Connecticut Department of Transportation (CTDOT) and Capitol Region Council of Governments (CRCOG) conducted a joint study, published in 2000, of the State Route 44 corridor through the Town of Avon. Among the recommendations were:

- Constructing sidewalks and a wide median along Route 44 – or possibly using service roads behind businesses through parking areas to connect pedestrians to commercial destinations
- Install crosswalks at all 12 signalized intersections on Route 44



Town of Avon Plan of Conservation and Development (POCD)

The town's last POCD was approved in 2016. Among its bicycle and pedestrian related recommendations were to:

- Promote the construction of new sidewalks
- Amend Zoning Regulations to clarify when private developers are required to install sidewalks in association with site plan approval for commercial projects
- Pursue with the CTDOT the installation of pavement markings and handicap ramps at all signalized intersections on Route 44
- Conduct a detailed evaluation of existing roadways and their suitability for on-road cycling, building on the 2012 study prepared by Chris McCahill, Ph.D.
- Evaluate roadways in association with the town's Pavement Management Program and determine if lane markings may be adjusted to better accommodate bicycles

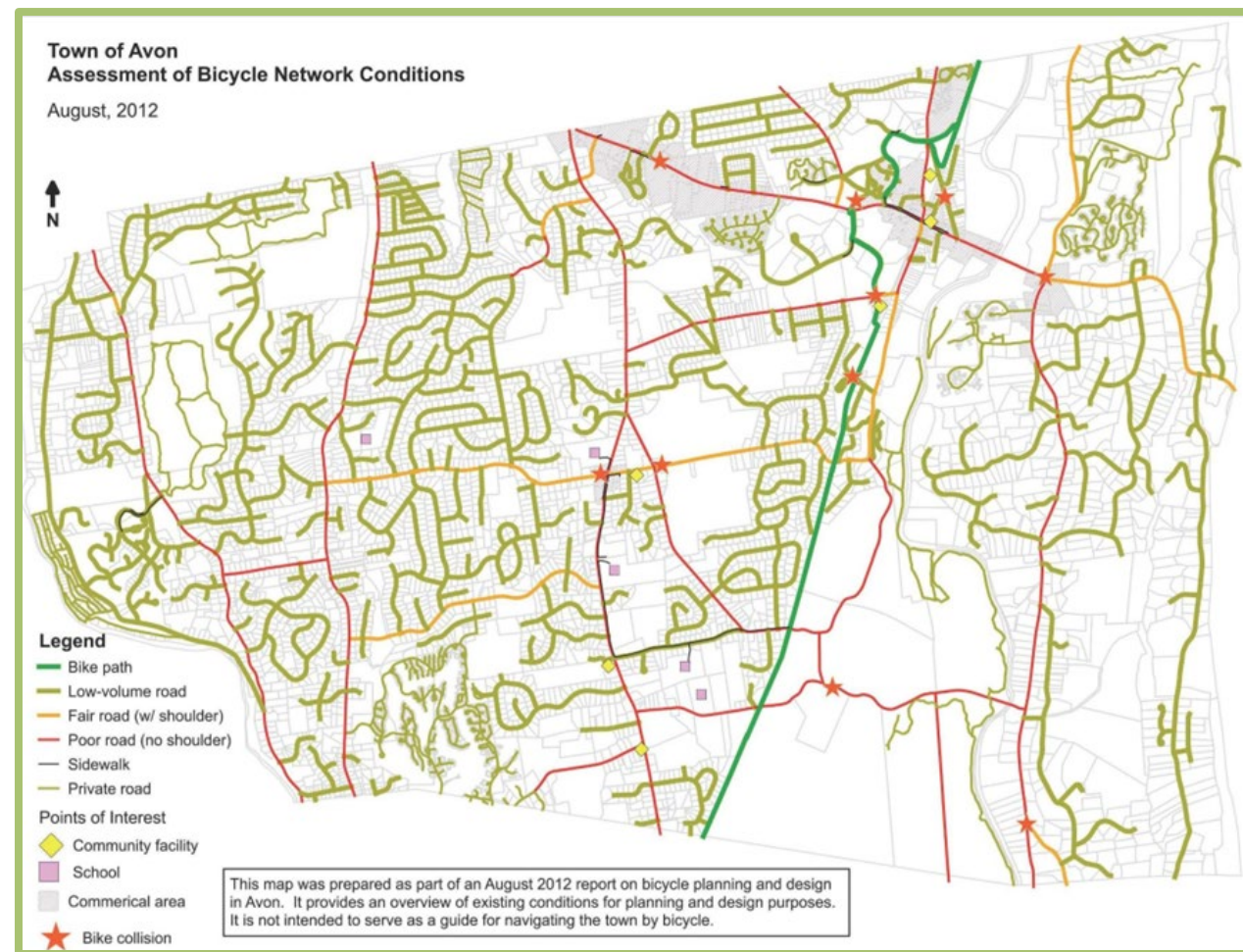


Figure 3: Town of Avon Assessment of Bicycle Network Conditions

Note: Full-sized map in Appendix A

1.4 Existing Conditions

Existing conditions for bicyclists, pedestrians, and transit riders are documented to form a basis from which the assessment and prioritization of facilities for all users of the transportation system can be made. Many factors must be considered when developing a safe network for bicyclists and pedestrians – including past present and future plans; the existing transit network; existing recreational and functional facilities like trails, sidewalks, crosswalks, bicycle parking, and signage; the location and circumstances around bicycle and pedestrian crashes; Right of Way (ROW) and other physical constraints; and both the *posted* and *actual* travel speeds on local roads.

This map from the town's Recreation and Parks website shows the Farmington Canal Heritage Trail's North/South orientation through Avon, connecting to Farmington and Simsbury - and the many excellent outdoor recreation areas dispersed throughout the community.

Because of its importance as a beloved local recreation area, a connection to Reservoir 6 on Avon Mountain warrants further study - as does the possibility of an alternate connection through Fisher Meadows park - which could potentially be considered as part of Avon's Recreation and Parks Master Plan.

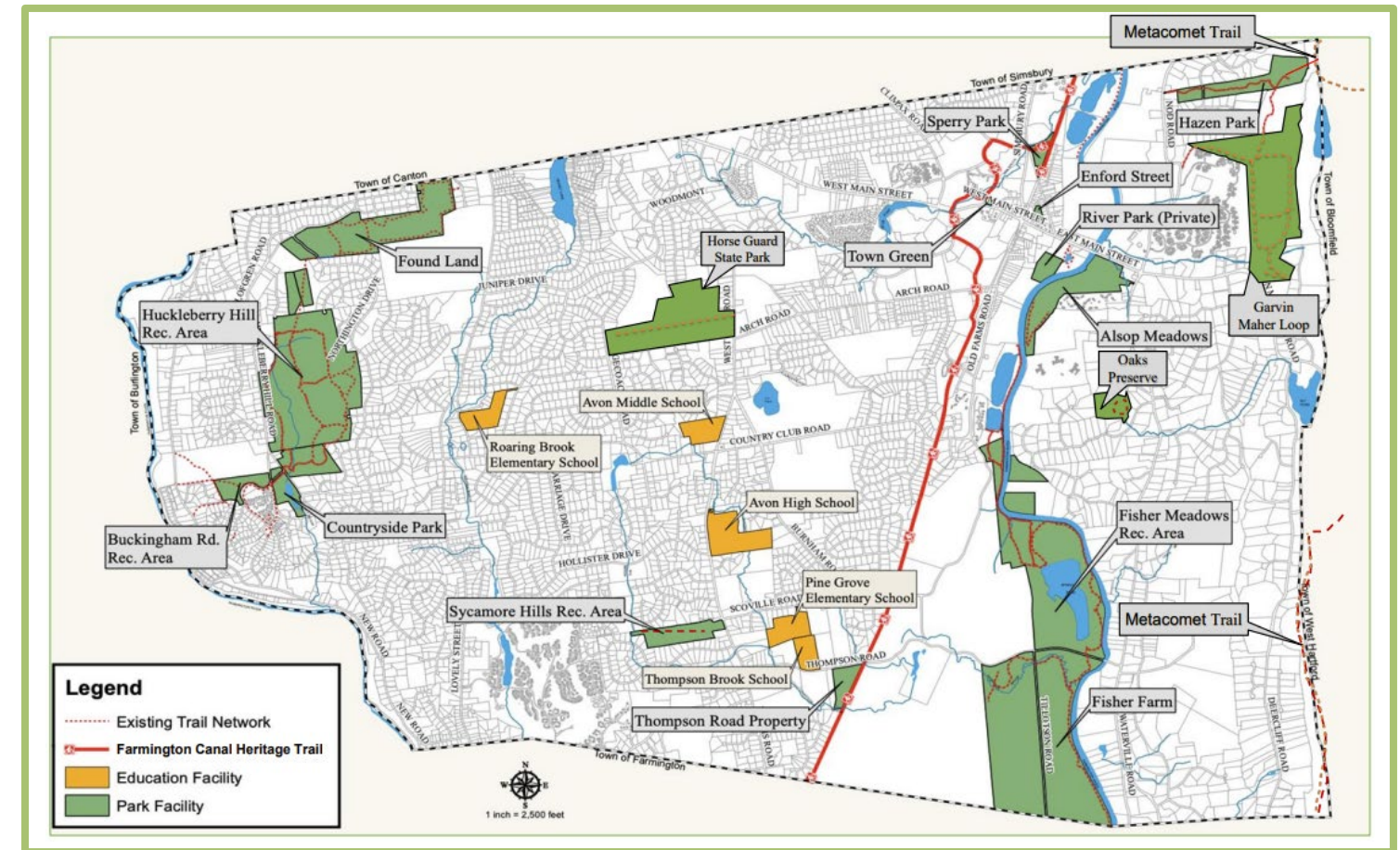


Figure 4: Town of Avon Recreation and Parks Map of Trails, Education Facilities, and Parks



1.4.1 Walking

In addition to the recreational walking facilities within its park and trail system, and the Farmington Canal and Metacomet Trails, Avon has some sidewalks on Scoville, West Avon, Country Club, East and West Main, and Old Farms Roads, as well as radiating out from Old Avon Village and the Town Center, and in neighborhoods off Nod and Huckleberry Hill Roads. Crosswalks, trailside benches, and trail access points were also mapped to help identify the gaps in existing facilities.

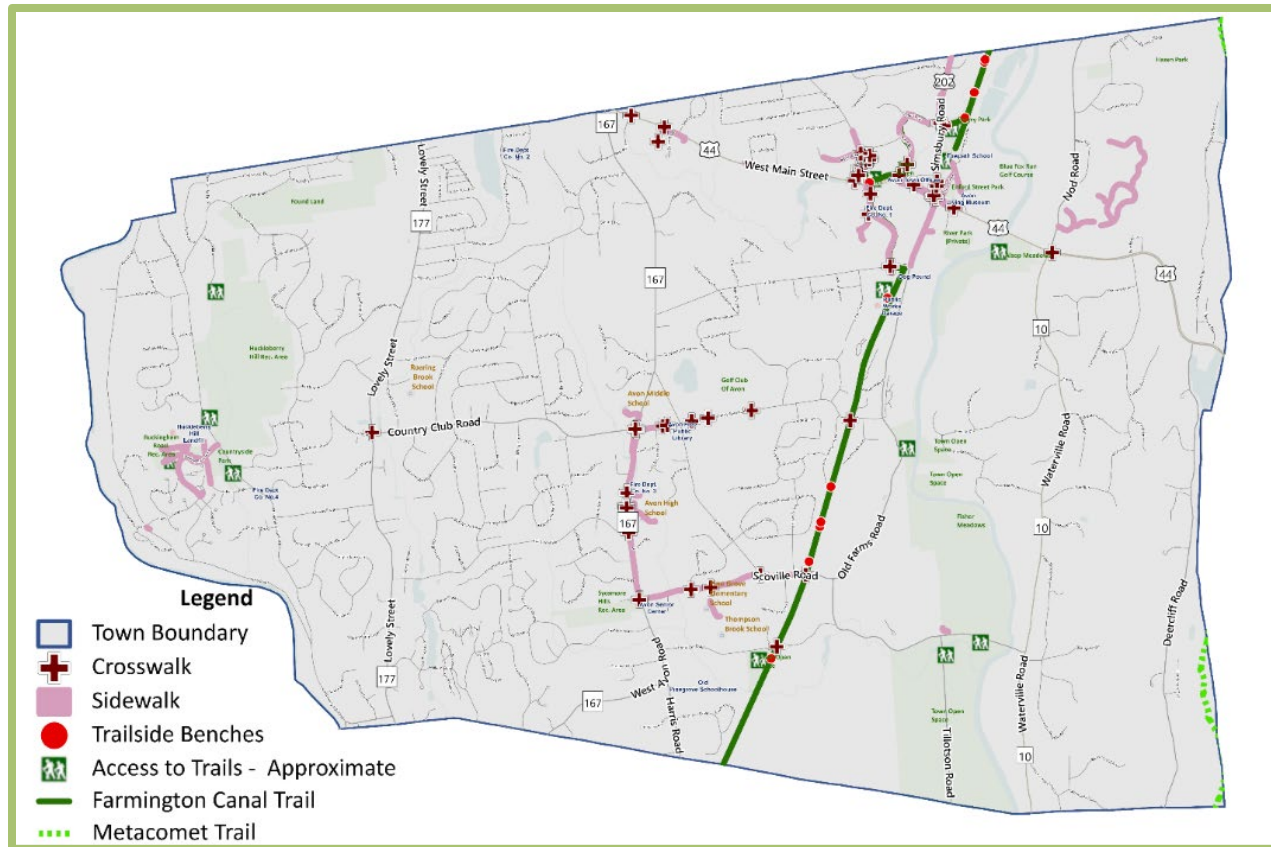


Figure 6: Avon's Walking Infrastructure

Note: Full-sized map in Appendix A

1.4.2 Bicycling

Apart from bike racks in about 20 locations, and trailside benches, the Farmington Canal Heritage Trail is the only other physical amenity for bicyclists in Avon. The Farmington River Trail is a very popular bicycling trail as well, located over the town's border in Burlington, Farmington, and Canton.

1.4.3 Crashes

Based on review of the University of Connecticut Crash Data Repository, there were nine pedestrian collisions in Avon—including two fatalities—that occurred between April 2018 and April 2023. In that same time period, five bicycle collisions and one "other non-motorist" collision were reported. To understand what can be done to prevent future crashes, planners try to find



Pedestrian & bike collisions in Avon (April 2018 - April 2023)

trends. When it comes to the two fatal crashes in Avon, both were pedestrians and both were in the cold months of November and February, but that's where any similarity ends. One was during the daylight hours – in rainy conditions; one was in the very early morning when it was still dark.

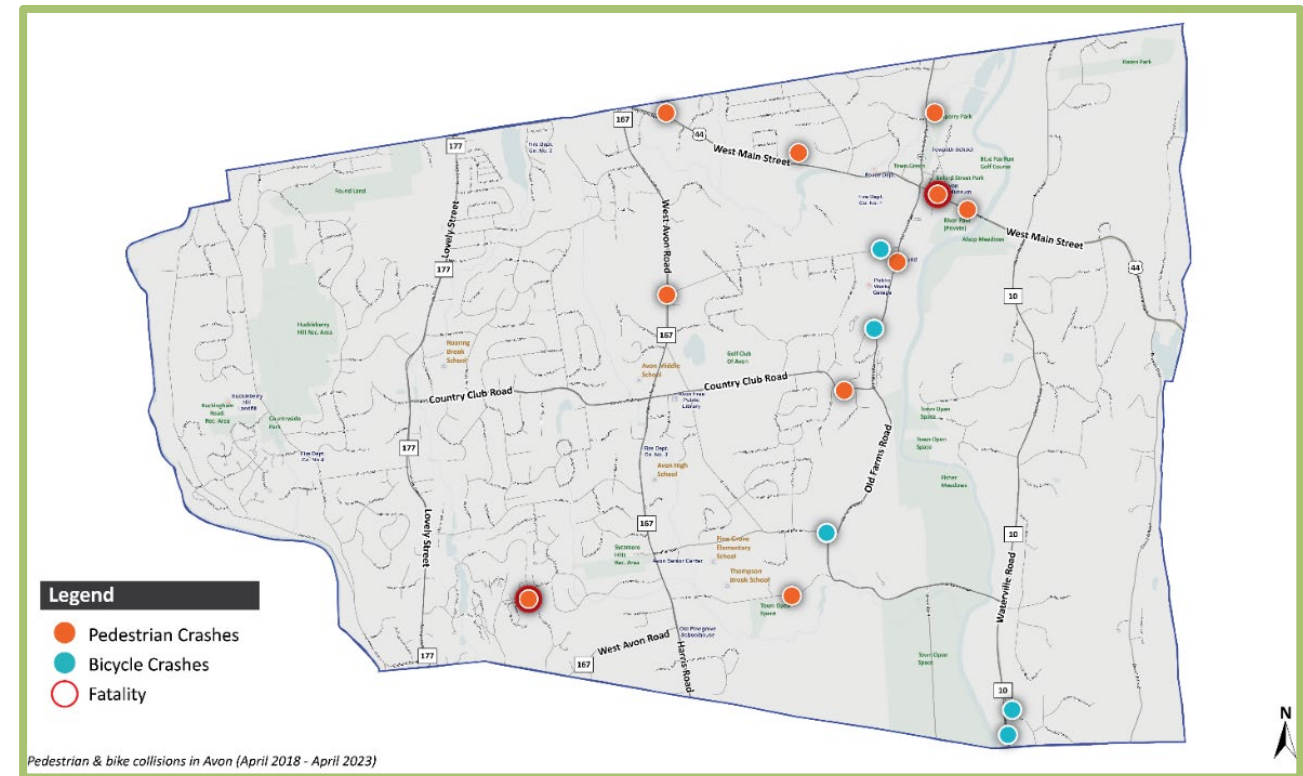


Figure 5: Pedestrian and Bicycle Crashes in Avon from 2018 to 2023

Note: Full-sized map in Appendix A

1.4.4 Transit

An important factor impacting bicycling and walking is the availability of transit, which facilitates multimodal travel when trips are otherwise too long for biking or walking. The CTtransit buses are equipped with bike racks, and the following are CTtransit's routes through Avon:

- 901 (Avon/Canton Express)
- 926 (Winsted Express)
- 927 (Torrington Express)

All routes travel along East and West Main Street (Route 44) and provide weekday service only – primarily during morning and evening commute times.

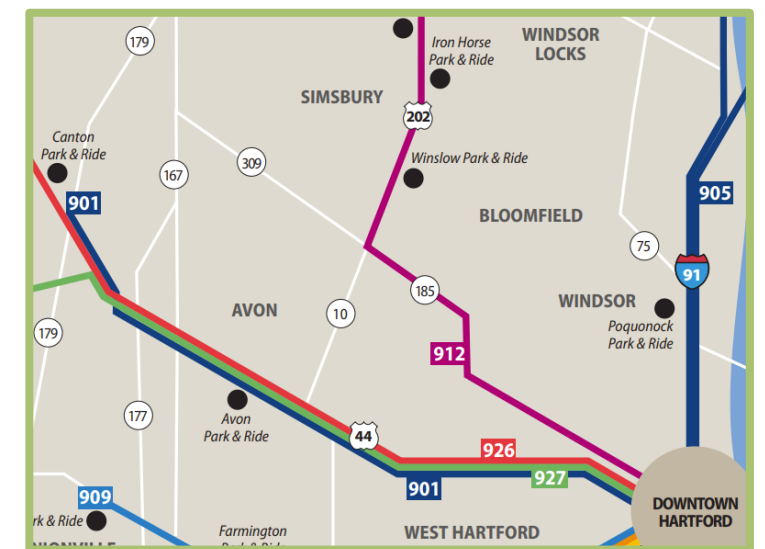


Figure 7: CTtransit Routes between Avon and Hartford

Source: https://crocog.org/wp-content/uploads/2018/05/Express_system_0.pdf



Table 1: Inbound CTtransit Bus Schedule

Inbound to Hartford	A.M.					P.M.			
	West Main Street and Dale Road	6:12	6:42	7:12	7:42	8:13	2:28	4:38	5:47
West Main Street at Shops at Nod Brook	6:13	6:43	7:13	7:43	8:13	2:28	4:38	5:48	6:33
West Main Street at Walmart/Big Y Plaza	6:14	6:44	7:14	7:44	8:14	2:29	4:39	5:49	6:34
Avon Park and Ride-Walmart/Big Y Plaza	6:15	6:45	7:15	7:45	8:15	2:30	4:40	5:50	6:35
West Main Street and Old Farms Road	6:19	6:49	7:19	7:49	8:19	2:34	4:44	5:54	6:39
East Main Street at Avon Citgo	6:19	6:49	7:19	7:49	8:19	2:34	4:44	5:54	6:39

Note: Inbound (to Hartford) stops in Avon – **p.m. stops in bold**

Table 2: Outbound CTtransit Bus Schedule

Outbound to Canton/Torrington	A.M.					P.M.			
	East Main Street at Fairway Shops	5:53	6:08	7:33	12:44	4:05	4:35	5:06	5:35
West Main Street & Simsbury Road	5:54	6:09	7:34	12:45	4:06	4:36	5:07	5:36	6:07
Avon Park and Ride-Walmart/Big Y Plaza	5:58	6:13	7:38	12:50	4:11	4:41	5:12	5:41	6:12
West Main Street at Goodwill	5:58	6:13	7:38	12:50	4:11	4:41	5:12	5:41	6:12
West Main Street at M&T Bank	5:58	6:13	7:38	12:50	4:11	4:41	5:12	5:41	6:12
West Main Street at Avon Marketplace	5:59	6:14	7:39	12:52	4:13	4:43	5:14	5:43	6:14

Note: Outbound (to Canton/Torrington) stops in Avon – **p.m. stops in bold**

Free parking to use the bus is available at the Avon Park & Ride and at the Walmart Plaza. Currently, there is only one bus shelter in Avon – at the Walmart Plaza.

To encourage use of the bus, sidewalks and crosswalks should be provided to allow safe and convenient access to Avon’s bus stops, and shelters should be provided to protect bus riders from the elements. The Town of Avon has recently applied for a *Small Town Economic Assistance Program (STEAP)* Grant to install bus shelters at 5 locations on the south side of Route 44. Although not successful with the 2023 request, they will continue to pursue the funding for this project.

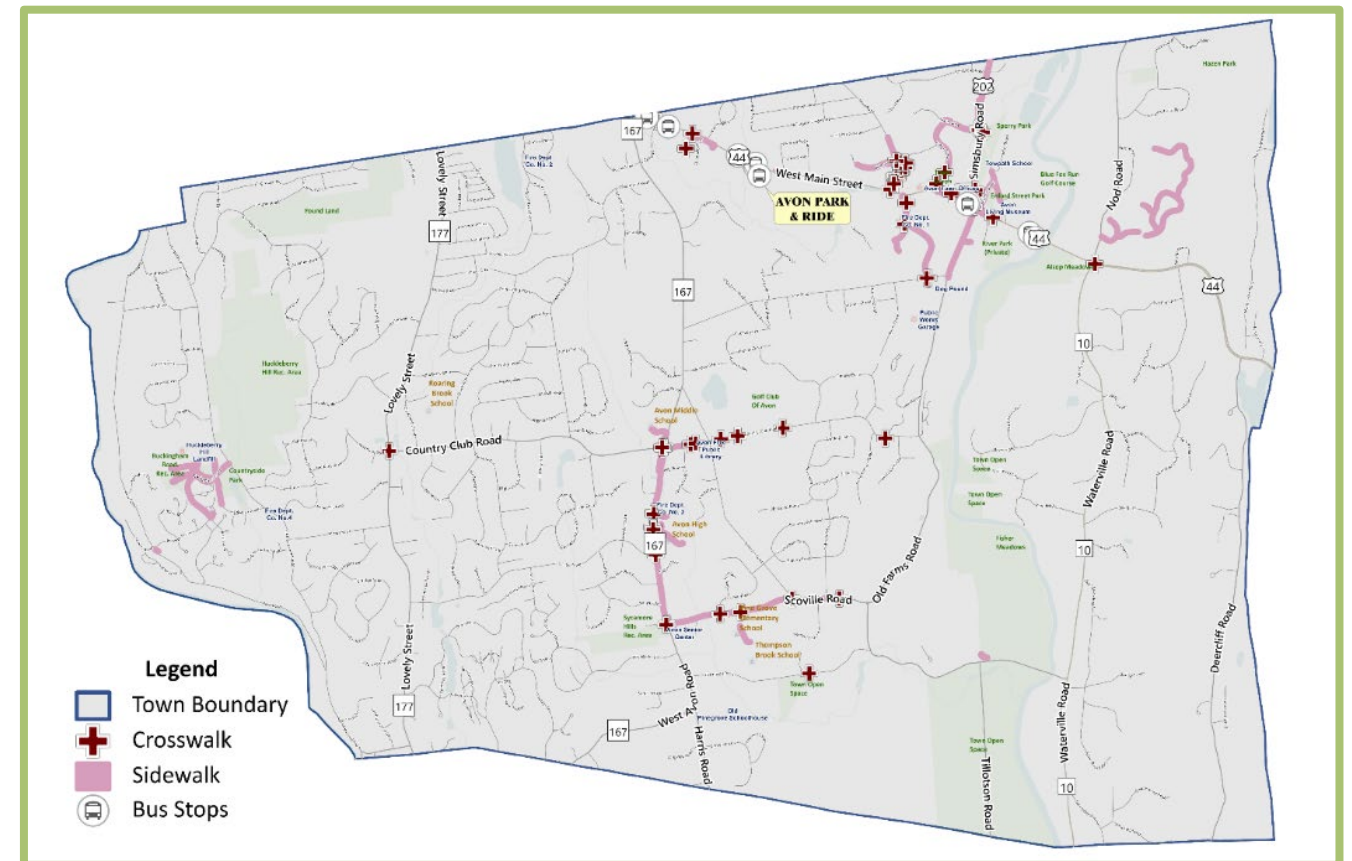


Figure 8: Avon’s Transit Infrastructure

Note: Full-sized map in Appendix A



Figure 9: Typical Bus Stop in Avon

Note: This bus stop on Route 44 does not have sidewalk leading to it, or a sheltered place to sit and keep a bus rider protected from the elements.



1.4.5 Speed

Vehicle speed has everything to do with safety and quality of life. Although most of Avon’s roads are posted at 40 miles per hour (mph) or less, actual traffic speeds are higher, and even **at 40 mph, only 1 out of 10 pedestrians struck by a car traveling at this speed will survive**. The speed we travel on foot is about 3 mph. By bicycle we travel around 10 to 12 mph. It’s that significant differential that makes us feel unsafe when we aren’t driving, and that induces most pedestrians and cyclists to drive a car - unless they have a place to walk or bike that is fully separated from speeding traffic.

Table 3: Posted Speed Limits on Avon’s Roads

25 mph	30 mph		35 mph	40 mph	45 mph
Burnham Road (West Avon to Country Club Road)	Arch Road	New Road	Country Club Road (Tamara Circle to West Avon Road)	Avon Mountain Road	
Carriage Drive	Burnham Road (Country Club to Scoville Road)	Nod Road	Darling Drive	Lovely Street	
Climax Road	Chevas Road	Old Farms Road, West Main Street to Country Club Road	Huckleberry Hill Road, Deer Run to Canton Line	Simsbury Road	
Deercliff Road	Country Club Road (west)	Scoville Road	Tillotson Road	Waterville Road (East Main Street to Pheasant Run)	Waterville Road (Pheasant Run to Talcott Notch)
Huckleberry Hill Road, Farmington line to Deer Run	Country Club Road (Old Farms Road to Tamara Circle)	Security Drive		Waterville Road (Talcott Notch to Farmington town line)	
Juniper Drive	Harris Road	Stagecoach Road			
Old Farms Road (Country Club Road to Waterville Road)	Hollister Drive	Woodmont	West & East Main Street	West Avon Road	
Orchard Street					
Thompson Road					

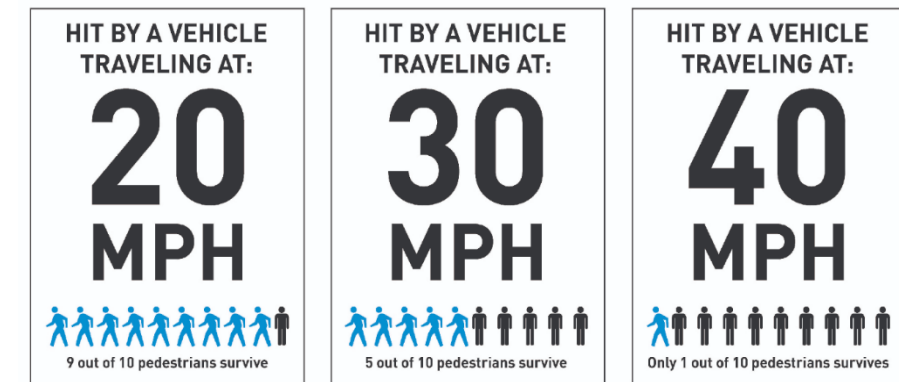


Figure 10: Effect of Vehicle Speed on Pedestrian Safety

Source: https://sdotblog.seattle.gov/2021/01/27/vision_zero/

1.4.6 Right of Way

In order to determine the feasibility of adding facilities to a roadway, the public Right of Way (ROW) must be known. The ROW includes the paved portion of the street, as well as the area outside of the pavement that is required to accommodate traffic signs, plowed snow, utilities, and sidewalks. The ROW is owned by either the town or the State. The width of the ROW can vary by street but the *standard in Avon for local roads is 50 feet, and the ROW on collector roads is 60 feet*. Avon’s ROW limits – as shown on the map below (Figure 11) are not considered a significant constraint to adding bicycle or pedestrian facilities, as

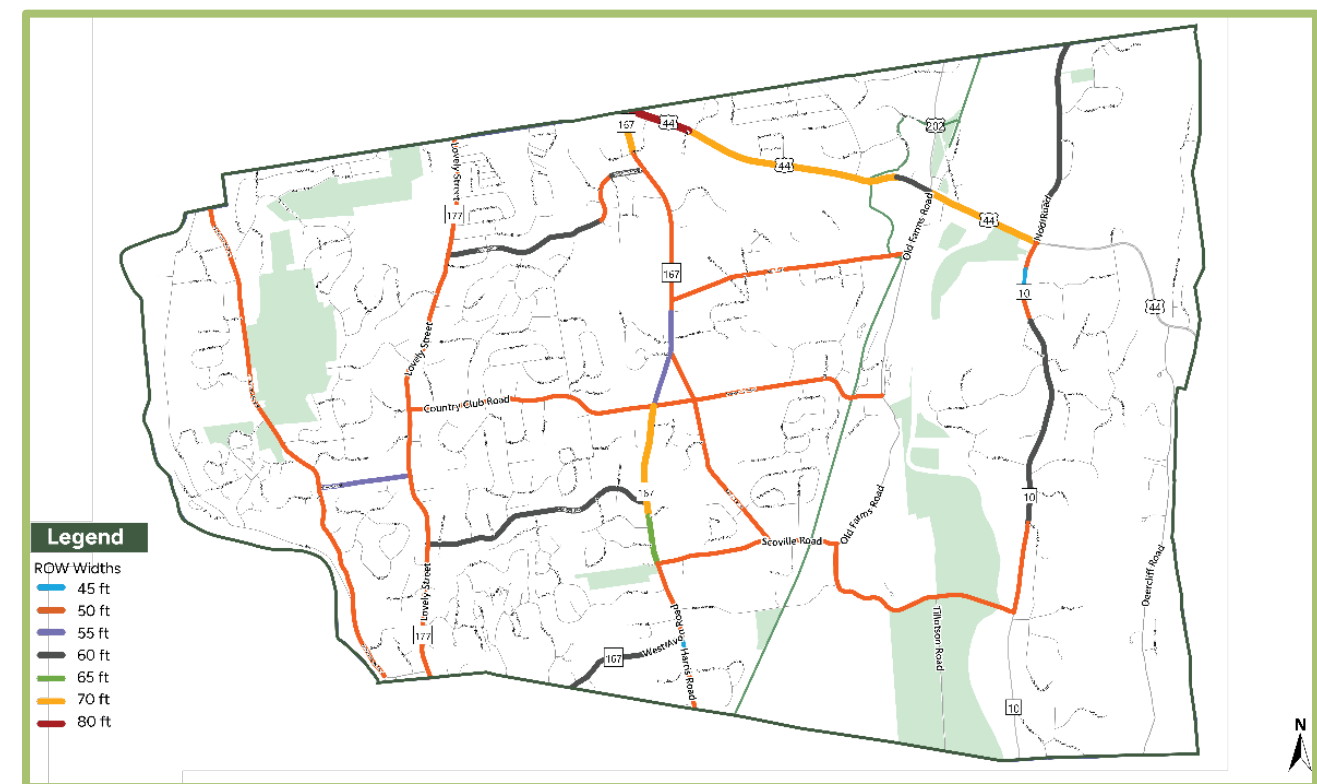


Figure 11: Right of Way Widths on Avon’s Roads



the ROW is generally wide enough to accommodate the potential improvements recommended as part of this study. For those areas where the bicycle/pedestrian recommendations are located outside the ROW, the town will need to pursue the acquisition of property rights from the landowners, which can extend project timelines or create challenges to implementing a project.

2.0 Outreach

Public outreach for this project came in several forms:

- Meeting with Bike Walk Avon, CT and with Avon residents at a Monday Minds and Meals event at the Senior Center
- Responses to Monthly Mobility Questions – accessible through the project’s StoryMap website
- Comments received on map boards available at the library and at the Senior Center
- Comments received at the three public meetings, held at the Senior Center on April 25, June 20, and September 19, 2023.

A compilation of all comments can be found in **Appendix B – Outreach Materials**

2.1 Monthly Mobility Questions

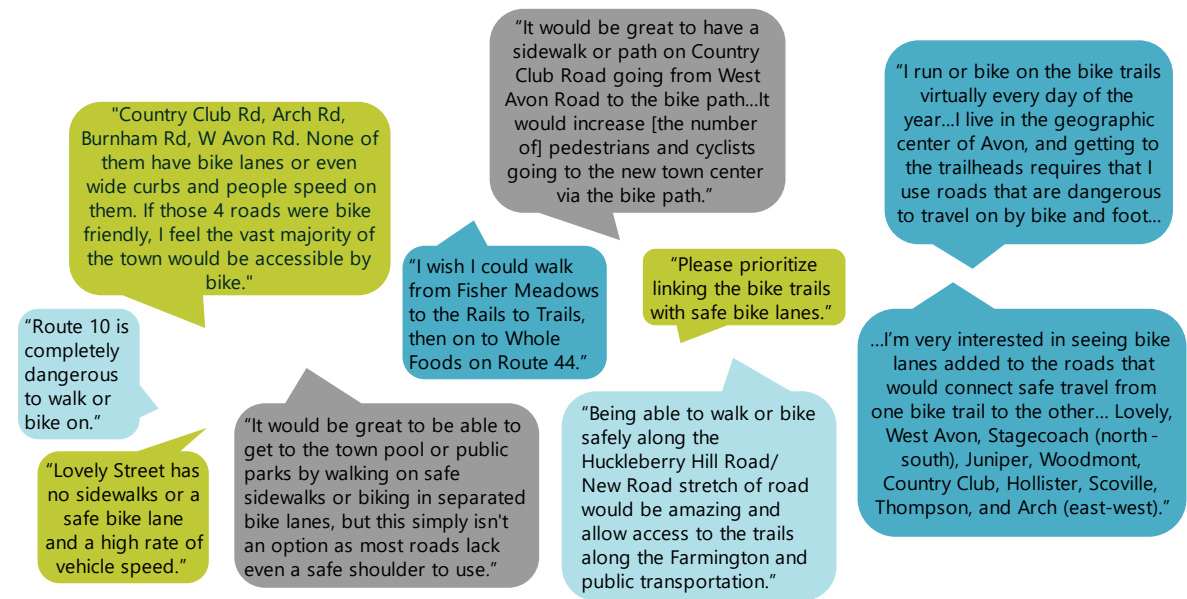
As mentioned in the Project Background, a series of short survey questions, designed to draw out the public’s thoughts and concerns about walking and bicycling conditions in Avon, were posted on the Bicycle and Pedestrian Plan’s StoryMap website and publicized through The Town of Avon’s email updates and Facebook page.

Monthly Mobility Question #1

Where do you wish you could walk or bike safely?

The first Monthly Mobility Question asked Avon residents if there are places in town where they feel unsafe walking and bicycling – and if so, to indicate where those places are on a map. One hundred thirty people responded to this question, showing on a map where they would like to be able to walk or bike safely. The most frequently mentioned areas include the intersections of Country Club Road and Lovely Street, West Avon Road, Burnham Road, Old Farms Road, the segment of Country Club Road between Stagecoach Road and Highwood Drive, Lovely Street near the intersections of Parkview Drive and Craigemore Circle, and Route 44/West Main Street near the intersection with Climax Road. There were 22 comments about the library – clearly a location the community cares about deeply. A sampling of the responses is shown in **Figure 12**.

All of the comments were geolocated and summed. In **Figure 13**, a graphic depicting the areas of interest to residents is provided. The larger the circle, the more responses were provided for that location.



Monthly Mobility Question #2

Do you have school-age children? If so, how do they get to school?

Eighty-seven people responded to this question, and of those, 62 people indicated that they do have school-aged children and provided details on their child (or children’s) mode of transportation to school. In **Figure 14** it can be seen that the vast majority of children are either driven to school, or travel by bus. A sampling of some of the comments are depicted as well.

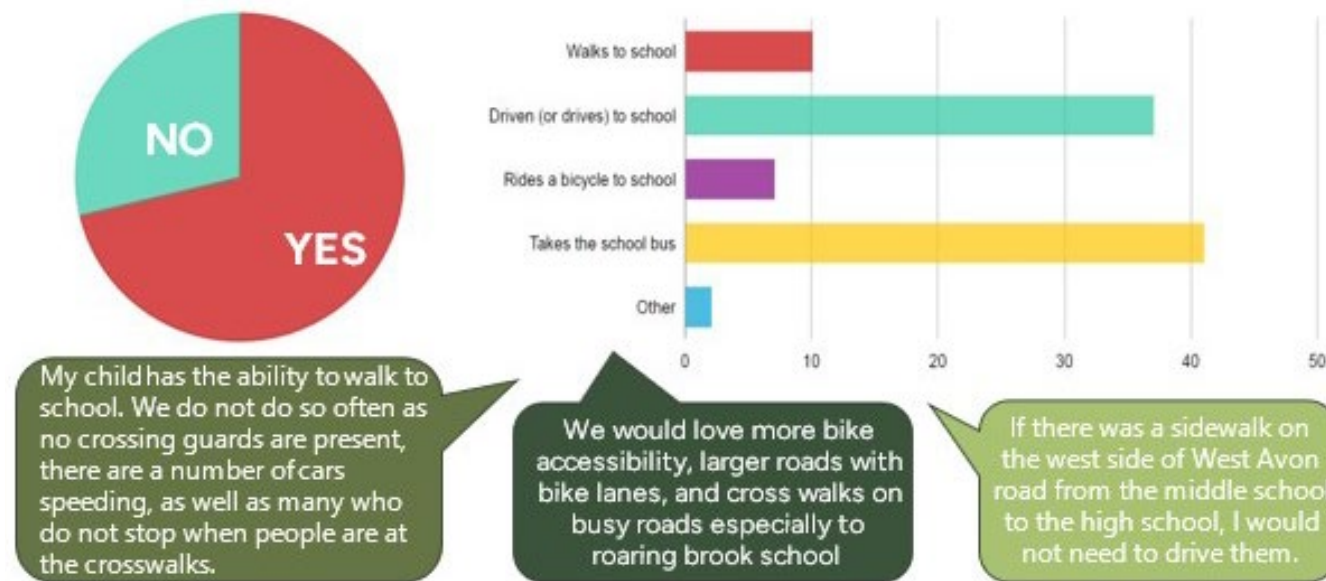


Figure 15: Responses to MMQ #2

Monthly Mobility Question #3

Do you support the addition of sidewalks in your neighborhood?

The question included the caveat that “sidewalks would primarily be installed in the public ROW, and that you as the property owner, would be responsible for the maintenance of the sidewalks, including but not limited to snow removal – in accordance with town ordinance.” There were 76 responses to the question, and some of the written comments are shown above. Those in favor of sidewalks outnumber those opposed by an over 5 to 1 margin. The results of this question are shown graphically in **Figure 15**, and a sampling of comments are depicted.

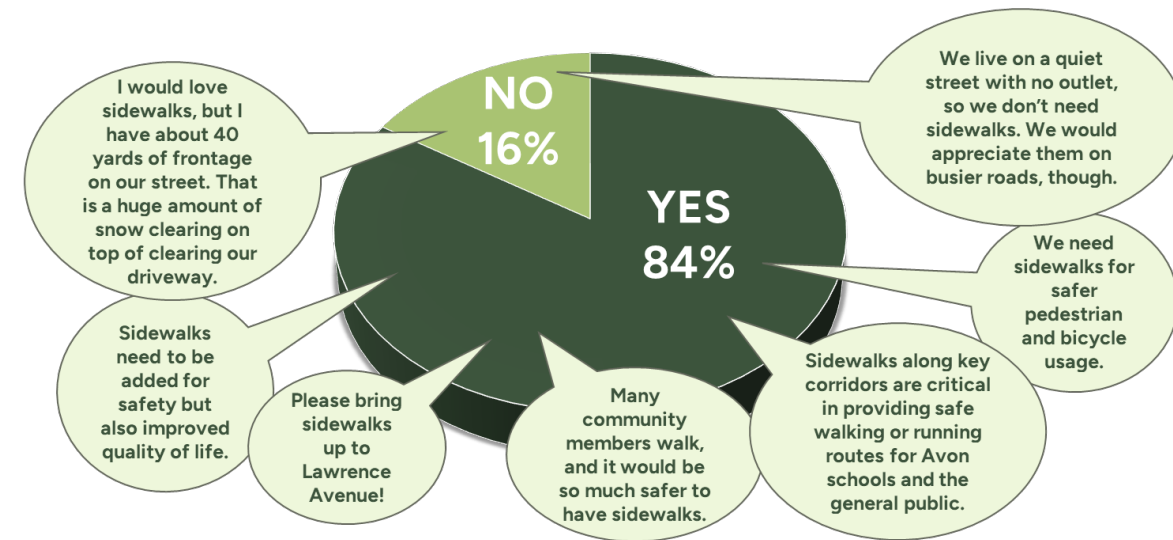


Figure 14: Responses to MMQ #3

Monthly Mobility Question #4

To what degree are you satisfied with the following issues and services in the Town of Avon?

There were 68 responses to the fourth Monthly Mobility Question. As shown in **Figure 16**, most respondents have a negative, or neutral opinion on mobility resources in town.

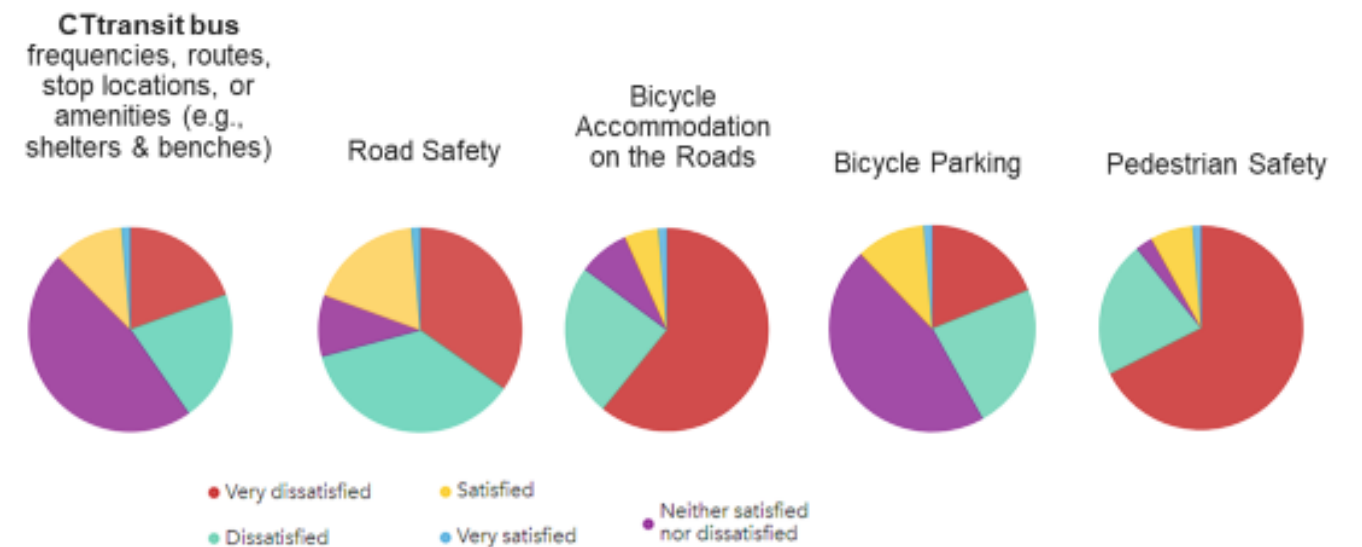


Figure 16: Responses to MMQ #4



2.2 Public Meetings

2.2.1 Senior Center Presentation

A public presentation was made at the Avon Senior Center's *Monday Meals and Minds* series on March 13, 2023. Several people in the audience identified as bicyclists and a few as pedestrians. The discussion included the following comments:

- Scoville Road's sidewalk is in poor condition.
- The shoulder on Route 44 is narrow.
- Juniper Road has no sidewalks.
- The crosswalks at Route 167 near the Senior Center are "Crosswalks to nowhere".
- Dale Road and West Avon Road have no sidewalks.

2.2.2 First Public Meeting

At the first public meeting on April 25, 2023 – also at the Senior Center, a presentation was made showing the existing conditions for bicycling, walking, and transit use in Avon. The consultant team shared what they learned from maps, previous studies, field work, meeting with Bike Walk Avon, and compiled responses to Monthly Mobility Questions – accessible through the project's StoryMap website and on map boards at the library and at the Senior Center. The project's revised Vision Statement was also shared with the community.



Figure 17: Bike rack on CTtransit bus

Source: Google image search

through the Monthly Mobility Question and map boards. Lastly, SLR presented summaries of previous studies that relate to bicycle and pedestrian planning in Avon, and the project's next steps, followed by discussion.

Community Ideas and Comments from the First Public Meeting:

- Sidewalks should be wide enough for bicyclists and pedestrians to share.
- Signage – such as "share the road" may be a low-cost and implementable first step to create bike routes.
- Conflicts between bicyclists and pedestrians on the Farmington Canal Heritage Trail make it challenging to use in the peak season.
- The project should share and recommend the best practices in creating safe routes to schools.
- Bike buses are a possible solution – with police escort.
- Funds are available from the U.S. Department of Transportation through the "Safe Streets for All" grant program.
- E-bikes' speed must be addressed, as they travel up to twice the speed of a pedal bike and significantly faster than pedestrians – policies must address where and how they can safely integrate into the transportation and recreation network.
- Short-term/quickly implementable solutions should be proposed.
- Route 44 creates a barrier in the town's primary activity center.
- "Bicycle Superhighways" are being built in Europe.
- Burnham Road should be prioritized – as it connects schools.
- There is no safe parking area at the trailhead to Heublein Tower on Nod Road.
- Traffic calming can and should be done with measures such as chicanes, flex posts, and speed cameras – a "Vision Zero" bill is pending that may make some of these solutions more practiced/available.

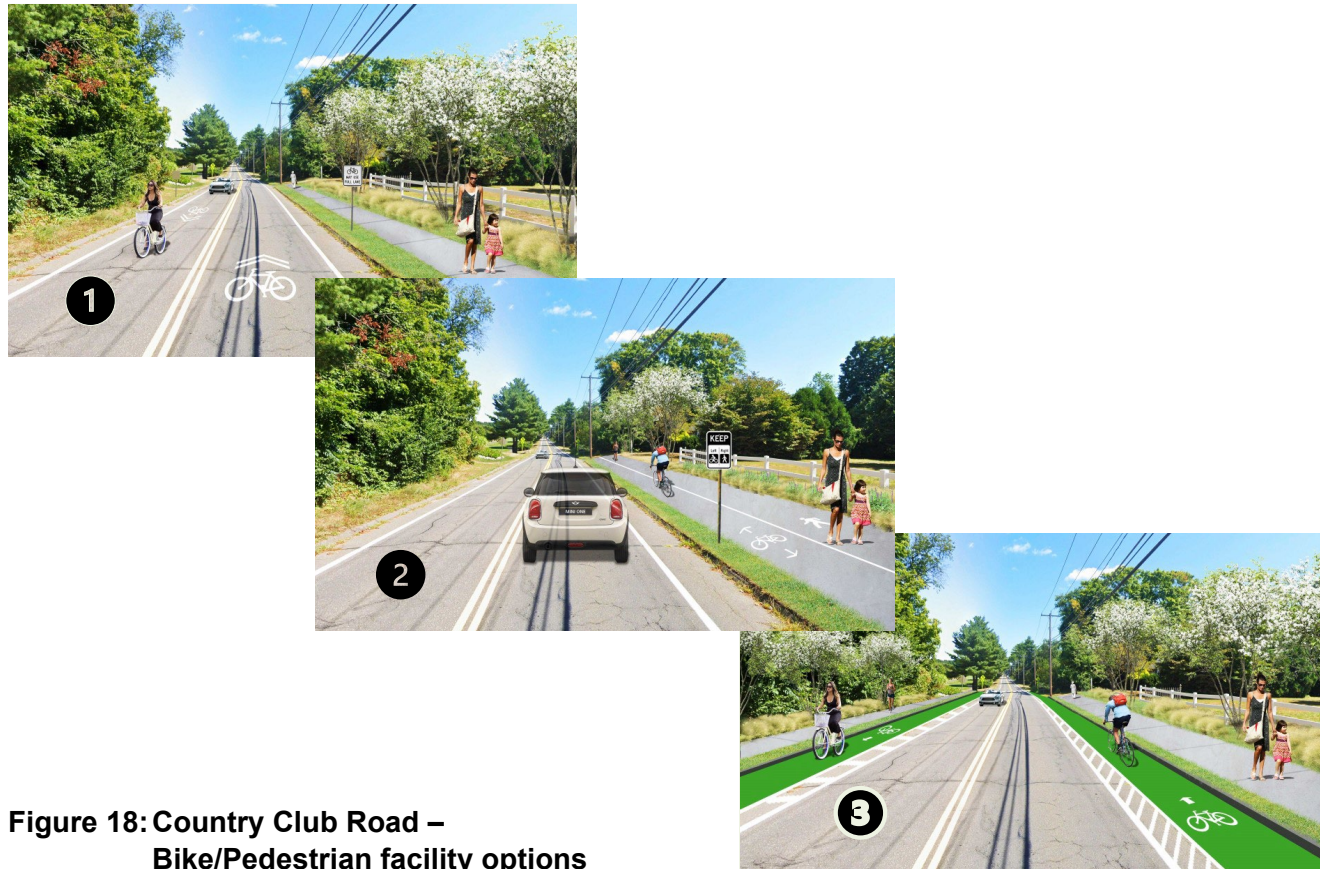
2.2.3 Second Public Meeting

A second public meeting was held at the Avon Senior Center on June 20, 2023. The SLR consultant team shared the research, further analysis of public comments, and work with the town's staff advisory group that led to an understanding of the opportunities and constraints to improving Avon's roads for walking and bicycling. It also was an opportunity to share the recent news that the Town Council passed a Complete Streets resolution and was awarded Bronze level Bicycle Friendly Community status.

This presentation was largely about how decisions about new investments in bicycle and pedestrian infrastructure and amenities can be guided. It covered how the roadways in Avon could better connect to each other with bicycle and pedestrian facilities, what implementation looks like – including policies that could change through zoning and ordinances, and what kind of improvements could be made in the short and longer term. With all of that background, participants were asked about their preferences – weighing technical and personal factors.



Specifically, they were asked which of the following three options (**Figure 18**) were most preferable as bicycle and pedestrian accommodations for Country Club Road:



**Figure 18: Country Club Road –
Bike/Pedestrian facility options**

Note: Full-sized options in Appendix B

Participants overwhelmingly chose **Option 2** because it provides facilities for all users while maintaining the more rural character of the town.

Community comments and ideas from the second public meeting included consideration of:

- Trailblazing guidance/signage for Farmington Canal Heritage Trail (especially from Sperry Park to Whole Foods)
- The effect of narrowing lanes on general safety
- Consideration of Climax Road as part of the Secondary Bike Network
- Wayfinding
- Review Bike Plan from 10 years ago (reference to map created by Avon native Chris McCahill, which is included in Avon’s POCD)
- Connection to Unionville
- Wetlands as a constraint
- Use Stagecoach Road for a North/South connection

2.2.4 Third Public Meeting

At the request of Avon residents, a third public meeting was held on September 19, 2023, at the Avon Senior Center, so that the proposed recommendations for bicycle and pedestrian infrastructure, policy, and signage could be shared with the community before being presented to the Town Council.

The project’s timeline was reviewed, as was a slide showing the significant progress made since the project’s inception, including the Town of Avon:

- Becoming a Bicycle Friendly Community;
- Obtaining preliminary approval of \$4 million to improve Country Club Road and install pedestrian facilities from the library to Old Farms Road;
- Passing a Complete Streets Resolution that provides a policy framework for improved bicycle, pedestrian, and transit accommodations;
- Developing a bicycle education program for fourth graders attending the Avon Public Schools;
- Choosing to allow road widths to be as narrow as 10 feet – where practical – on low traffic volume *Collector* and *Local* roads, calming traffic;
- Applying for a grant to install bus shelters at 5 locations on Route 44; and
- Improving sidewalk on West Avon Road from Sunnybrook Drive to 460 West Avon Road

Early in the presentation, a group of High School and Middle School students, the “Green Valley Volunteers” led by M. Gokalp Gokcen, came to the podium to share their vision to make Avon’s roads safer for walking and bicycling – especially Lovely Street (Route 167).





Figure 19: Alternate Gateway to Avon Sign

The majority of the presentation focused on the specific recommendations made for pedestrian and bicycle infrastructure improvements. The process for developing the infrastructure recommendations is described in the Concept Development Section (**Chapter 3.0**). The presentation finished with the policy, program, and signage recommendations that could improve safety and behavior, and begin to change the culture for road users regardless of travel mode – such as this sign (**Figure 19**) welcoming visitors to Avon’s “Walkable + Bikeable Community,” and reminding drivers to do so carefully.

Community Ideas and Comments from the 3rd Public Meeting:

- Provide separated bike lanes on Lovely Street (Green Valley Volunteers).
- Extend West Avon Road pedestrian improvements shown on Section 14 around the Roaring Brook School further north (St. Matthew Church at Greenwood Drive?).
- There should be a consistent design element.
- There are no sharrows recommended
- Connect neighborhoods on the east side of Route 10 to each other, to Fisher Meadows, and to the Magnet School.

2.3 Current and Pending Projects

Bicycle and pedestrian infrastructure projects that are in advanced planning stages and/or are funded create a foundation upon which new improvements can be made.

Annual Pavement Plan

The Town of Avon budgets for and prioritizes certain roads for repaving every year. When this occurs, the opportunity to apply painted roadway treatments such as fog lines, crosswalks, shared lane markings or bike lanes presents itself. For this study the roadways planned for repaving were considered, but ultimately could not be improved for better bicycle or pedestrian access due to inconsistent width of the roadway and/or the presence of guide rails. A positive outcome, however, is that the Town of Avon made the decision that the travel lane on **Collector** roads with 1,400 or fewer Average Annual Daily Trips (AADT) or

roads classified as **Local** may be as narrow as 10 feet, as shown in **Table 4**. This can provide more space for pedestrians and cyclists along the road edge, and in the best case, space for a multi-use side path that accommodates both. Narrowing the vehicle travel lanes also slows cars and trucks, calming traffic.

Table 4: Allowable Lane Widths on Avon’s Roads

Road Name	Functional Class	AADT	Allowed Lane Width
State roads (US Routes 44/202, Routes 10, 167, 177)	Arterial	-	11 feet
Talcott Notch Road	Collector	6,000	
Nod Road		5,400	
Old Farms Road		3,000	
Thompson Road		2,500	
Country Club Road		4,800	
Arch Road		4,100	
Harris Road		6,600	
Hollister Drive		2,900	
Woodmont Road		3,000	
Chevas Road		2,500	
Huckleberry Hill Road		3,000	
Stagecoach Road		1,400	
Juniper Road		1,700	
New Road	800		
Carriage Drive	450	9 feet or 10 feet	
All other non-State roads (e.g., Burnham Road)	Local		-

Country Club Road Local Transportation Capital Improvement Program (LOTICIP) Grant

The Town of Avon received preliminary approval for a proposed project on Country Club Road that would complete road improvements to Country Club Road including the construction of 6,340 linear feet (1.2 miles) of a pedestrian facility from the library east to Old Farms Road, near the Fisher Meadows Recreation Area.

Old Farms Road Project

The Old Farms Road – Phase I Project (North/South Section) will include the construction of a paved multi-use trail beginning on Scoville Road at the Farmington Canal Heritage Trail. The trail will continue to the intersection with Old Farms Road and proceed south along Old Farms Road to the intersection with Thompson Road. It will be supplemented by a future segment proposed as part of the future Old Farms Road – Phase II Project (East/West) which would proceed from the Thompson Road intersection, west



along the north side of Thompson Road back to the Farmington Canal Heritage Trail and create a loop trail approximately 1.25 miles in length. The future East/West component would also include a trail segment heading east along Old Farms Road where it would meet the town's extensive network of trails in the Fisher Meadows Recreation area.

3.0 Concept Development

A robust toolbox of strategies to accommodate, and improve safety for, non-motorists were reviewed. The existing characteristics of Avon's streets provided a context for the applicability of these strategies. Below we describe the select treatments incorporated into the conceptual recommendations.

3.1 Pedestrian Network

The Town of Avon has a limited pedestrian network. As discussed in **Chapter 1.3 Existing Conditions**, sidewalks are primarily concentrated around Route 44 and along some sections of West Avon Road. Some neighborhoods also have a limited sidewalk network.



The recommendations for expanding the pedestrian network were based on identifying points of interest and prioritizing pedestrian access to those points. The points of interest in Avon include commercial destinations (such as Whole Foods, Walmart, or Old Avon Village), public destinations (such as Town Hall or the library), trailheads and recreation areas (Fisher Meadows, Buckingham Park, Huckleberry Hill Recreation area), and schools. These points of interest formed the locus around which walksheds were created.

A walkshed is the area around a point of interest that is reachable on foot for the average person. The walkshed area used for this study was half a mile, which is about ten minutes of walking. Walksheds for a quarter mile, a five-minute walk, were also developed. **Figure 20** shows the points of interest and corresponding walksheds identified within Avon, along with the existing sidewalks, existing crosswalks, and potential upcoming projects that are expected to expand pedestrian connectivity within Avon.

As can be seen, these walksheds are largely concentrated in three different areas of Avon:

- 1 Downtown Avon: Several shopping centers, Town Hall, and various parks
- 2 Central/Southern Avon: Schools, public library, senior center
- 3 Western Avon: Trailheads, Huckleberry Hill Recreation area, Buckingham Recreational area

Once the walksheds were developed, each area was evaluated for potential pedestrian infrastructure improvements. The proposed pedestrian improvements were selected based on existing roadway conditions, available rights-of-way, and potential physical obstructions, such as the presence of guiderails, rock outcroppings, wetlands, or other obstructive features. The proposed potential improvements include installation of sidewalks to connect important points of interest to each other, the addition of crosswalks at key intersections, upgrades to existing signalized intersections to accommodate pedestrians, and installation of Rectangular Rapid Flashing Beacons (RRFB), among other recommendations. The following discussion provides an overview of the pedestrian improvements introduced throughout the town.

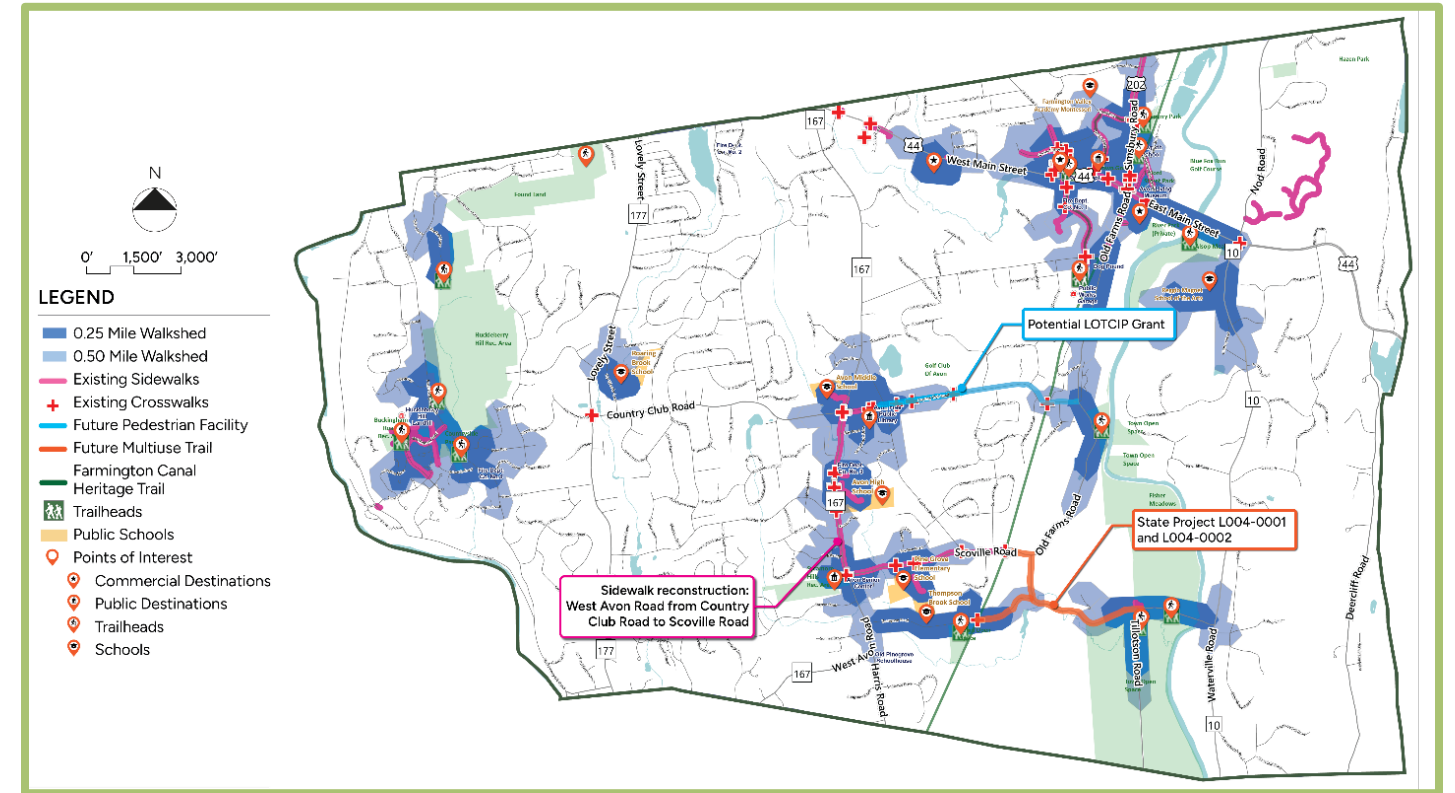


Figure 20: Pedestrian Walkshed Network

Note: Full-sized map in Appendix C

3.1.1 Sidewalks and Trail Extensions

Installation of sidewalks is one of the first steps in developing a more robust connected walking network and aids all pedestrians by giving them a designated safe place to travel. By prioritizing the installation of sidewalks in those areas identified by the walksheds, the town can help facilitate the safe movement of pedestrians between points of interest and, eventually, throughout all of Avon. Sidewalks separated from the roadway by a buffer zone (**Figure 21**) are the preferred accommodation for pedestrians and should be, at a minimum, five feet to comply with Federal Highway Administration (FHWA) standards. Per FHWA, any width less than five feet (or six feet where sidewalks are directly at the curb face) does not meet the minimum requirements for people with disabilities, although the American Association of State Highway and Transportation Officials (AASHTO) does permit four-foot sidewalks in certain contexts. For this study, sidewalks are suggested on both sides of Route 44 where feasible and on at least one side of the road throughout the rest of Avon within the walkshed areas.

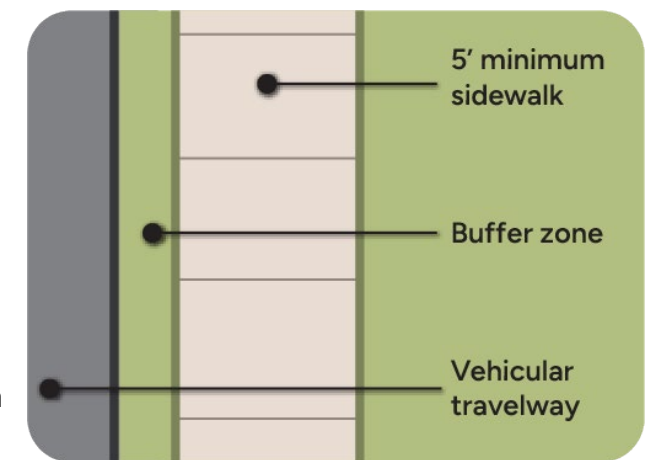


Figure 21: Preferred Sidewalk Treatment



In areas where sidewalk is not feasible, but pedestrians are in need of a designated facility, trail connections are suggested, such as within River Park between Old Avon Village and the gas station on Route 44 and on Huckleberry Hill Road south of Buckingham Road, where a “desire path” has already been worn by pedestrians looking to walk to or from the recreation areas.

3.1.2 Pedestrian Crossings, Ramps, and Detectable Warning Strips



Pedestrian crossings are designated places for pedestrians to cross a street, either at an intersection or midblock. Frequent, strategically placed crossings can help make pedestrian actions more predictable. Pedestrian non-compliance is often exacerbated by the lack of a direct route to desired destinations or excessive delay at controlled crossings. Marked crosswalks help guide pedestrians and alert drivers to a common crossing location, so visibility of the crossing for all users is key.

Curb ramps facilitate the transition from sidewalks to streets and should comply with the Americans with Disabilities Act (ADA) guidance and should be installed with detectable warning surfaces. For midblock crossings where pedestrians might be unexpected for approaching drivers, advanced warning signage should be provided to alert drivers of the potential crossing of pedestrians.

Crosswalks and ramps with detectable warning strips are recommended throughout Avon, particularly along Route 44 and on West Avon Road.

3.1.3 Rectangular Rapid Flashing Beacons

Rectangular Rapid Flashing Beacons (RRFB) are a pedestrian enhancement that warn vehicles when a pedestrian is looking to cross a roadway. They include two rectangular-shaped yellow indications, each with LED lights that flash when activated. RRFBs are recommended in several locations, such as on Huckleberry Hill Road and Old Farms Road, and are accompanied in our recommendations with installation of new crosswalks, ramps, and “pedestrian ahead” warning signage.



Source: www.tapconet.com

3.1.4 Signal Upgrades

At signalized intersections, pedestrian-specific signals should be integrated with the existing signal equipment and operations. This includes the installation of pedestrian signal heads on either side of a crosswalk and re-timing the signal to incorporate a pedestrian cycle – either exclusively when all vehicle signals are red and all pedestrian WALK signals are illuminated, or concurrently, when

pedestrians cross the road while parallel motorists have a green ball indication. During concurrent pedestrian phasing, turning vehicles are required to yield to crossing pedestrians. Throughout Avon, several intersections employ side street green phasing, where pedestrians share the green ball indications with parallel motorists, instead of having dedicated pedestrian signal heads. The side street green phasing can result in motorists thinking they have the right of way, while pedestrians are uncertain of when to cross. It is recommended that the signals utilizing side street green be upgraded to include pedestrian signal heads and any proposed future crossings include dedicated pedestrian signals as well.



Another feature that can be incorporated with concurrent phasing is a lead pedestrian interval (LPI). This provides pedestrians a “head start”, allowing them to enter the crosswalk area before the motorists get their green indication, where practical. The state has been including this feature into their pedestrian signal upgrades, and new traffic signals.

3.2 Bicycle Network

The existing bicycle accommodations throughout Avon are limited. Aside from bike racks and trailside benches, the Farmington Canal Heritage Trail is the only other physical amenity for bicyclists in Avon. The Farmington River Trail is a very popular bicycling trail as well, although it is not located with the town limits.

Connections for bicyclists traveling both locally and regionally were considered. The main east/west and north/south roadways within the town were identified and used to create a potential future network for enhancing bike connectivity throughout Avon and beyond, as shown in **Figure 22**.

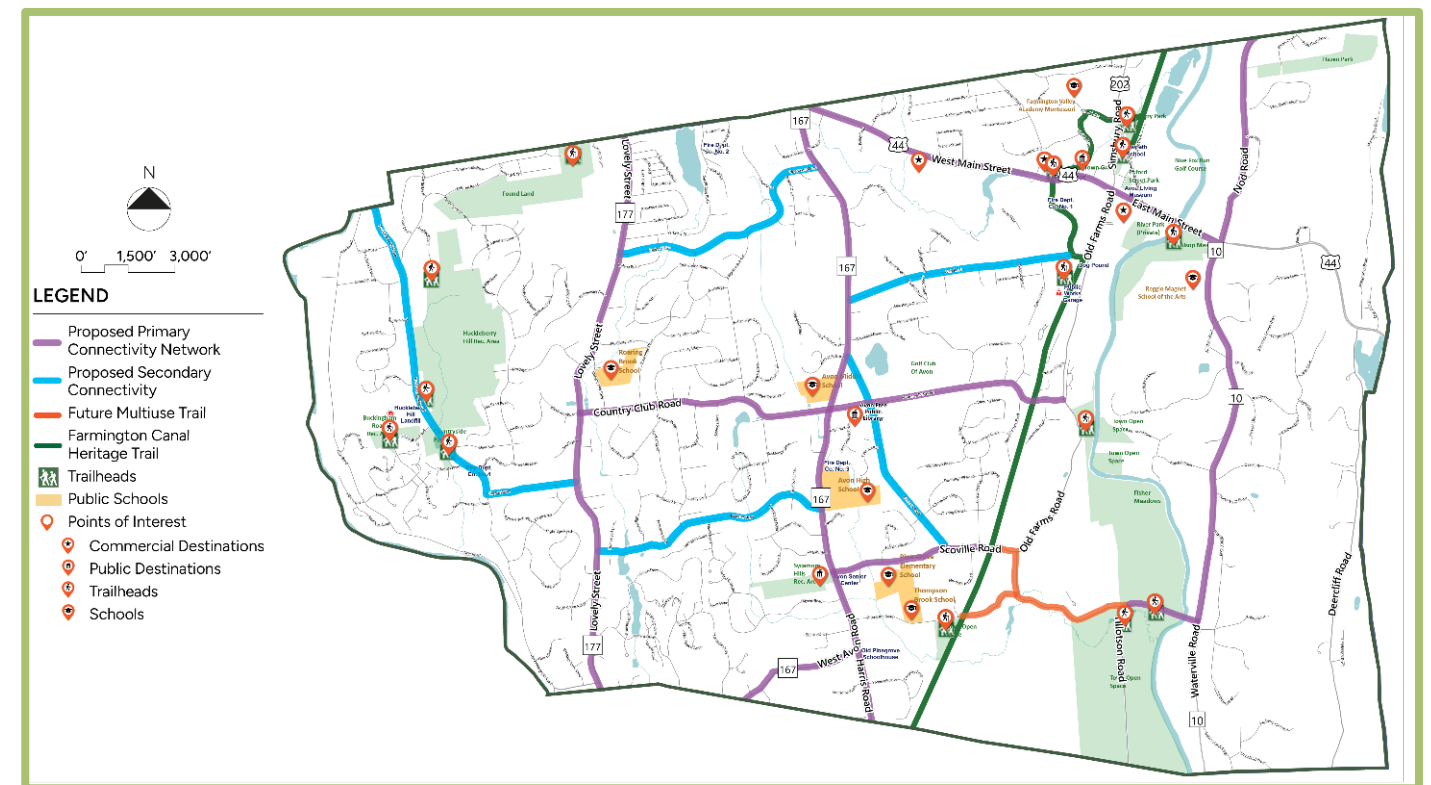


Figure 22: Bicycle Connectivity Network

Note: Full-sized map in Appendix C

The proposed network differentiates between primary (regional) and secondary (local) connectivity, where primary connectivity is largely regional and connects Avon to the surrounding towns, while secondary connectivity internally connects destinations within Avon. Figure 22 also shows the location of the Farmington Canal Heritage Trail, which will continue to be a key primary connection for cyclists and pedestrians alike, as well as future multi-use trails on parts of Thompson Road, Old Farm Road and Scoville Road. These future multi-use trails are part of current or pending projects within Avon. Taken together, they provide important connections that create north/south and east/west links through town.



Each primary and secondary road shown in the Bicycle Connectivity Network was then evaluated for potential bicycle infrastructure improvements. The recommendations rely on CTDOT’s new *Bicycle Facility Selection Guide* shown in Table 5 to determine the appropriate bicycle facilities based on traffic volumes and speed conditions, particularly on State roads, such as Lovely Street, West Avon Road, Route 44, Route 10, and Route 202.

Table 5: CTDOT Bicycle Facility Selection Summary Matrix

Traffic Volume (ADT)	0-5,000	5,000-10,000	10,000-18,000	18,000-20,000	20,000-25,000	25,000+
Paved outside shoulder	Recommended		Acceptable			
Bicycle lane	Recommended		Acceptable			
Buffered bicycle lane	Exceeds Recommendation	Recommended			Acceptable	
Side path	Exceeds Recommendation		Recommended			
Separated bicycle lane	Exceeds Recommendation			Recommended		
Posted Speed (mph)	0-25	26-30	31-35	36-40	41-45	46+
Paved outside shoulder	Recommended			Acceptable	Provisional	
Bicycle lane	Recommended			Acceptable	Provisional	
Buffered bicycle lane	Exceeds Recommendation		Recommended		Acceptable	Provisional
Side path	Exceeds Recommendation		Recommended			
Separated bicycle lane	Exceeds Recommendation			Recommended		

Source: Connecticut Department of Transportation. (2023). *Complete Streets Controlling Design Criteria and Justification Process*.

The proposed potential improvements include the installation of courtesy signage reminding vehicles of the potential presence of cyclists, bike lanes, bike lanes with buffers, and multi-use paths. The following discussion provides an overview of the bicycle infrastructure improvements recommended throughout the town.

3.2.1 Bike Lanes

Bike lanes are an exclusive space on the road designed especially for bicycles. They are typically on the right side of the road and provide a marked lane to show bicyclists where to travel. While bike lanes do create separation between riders and vehicles and can increase rider comfort or confidence on busy

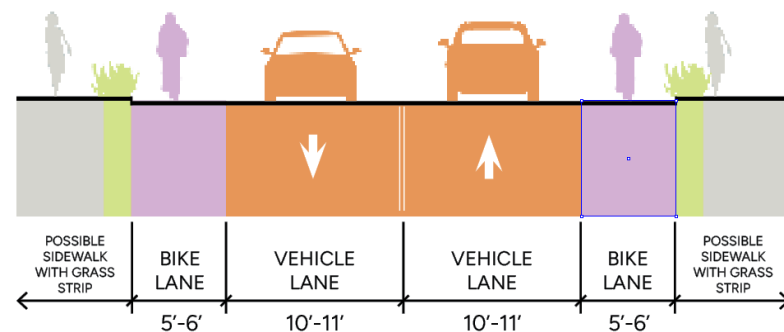


Figure 23: Bike Lane Cross-Section – Cross Section A

streets, the lack of a physical barrier between the bike lane and vehicular travel way can feel uncomfortable to some riders, especially on busy high-speed roadways. The desirable bike lane width is typically five feet at a minimum. Solid white lane markings are used to separate vehicular travel lanes from the bike lane, and bike lane symbols and arrow markings are used to define the bike lane. See **Figure 23**. This cross-section is referenced throughout the text, conceptual recommendation graphics, and Implementation Matrix as “Cross-Section A.”

3.2.2 Bicycle Lane Buffers

In addition to the solid white lane line markings typically used for a bike lane, a lateral offset with a painted buffer can help to further separate riders from vehicle traffic. The inclusion of bike lane buffers allows for more separation and provides increased maneuvering space; where appropriate and space permits, buffers should be added to provide a greater distance between vehicles and riders. See **Figure 24**. This cross-section is referenced throughout the text, conceptual recommendation graphics, and Implementation Matrix as “Cross-Section B.”

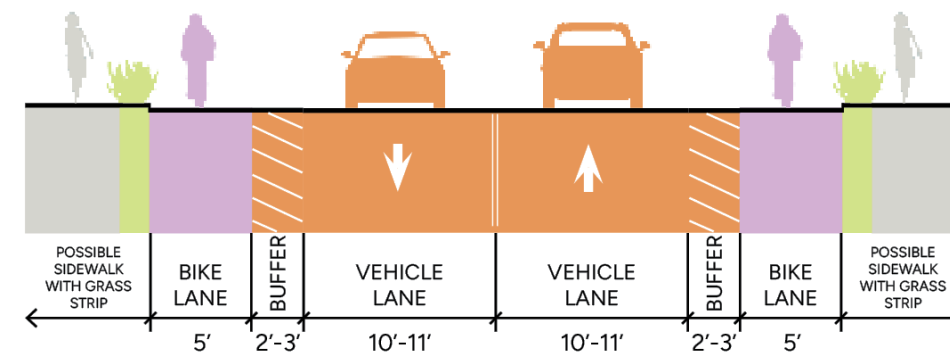


Figure 24: Bike Lane with Buffers – Cross-Section B



3.2.3 Multi-use Path

Multi-use paths provide a travel area for bicyclists, pedestrians, skaters, wheelchair users, and others separate from vehicular traffic. These facilities are also known as side-paths and are generally bi-directional, located adjacent and parallel to a roadway. Multi-use paths encourage bicycling and walking, among other non-motorized methods of transportation, in areas where high-volume and high-speed vehicular traffic would otherwise discourage it. The minimum width for a multi-use path is eight feet, although ten to twelve feet should be installed where possible. See **Figure 25**. This cross-section is referenced throughout the text, conceptual recommendation graphics, and Implementation Matrix as “Cross-Section C.”

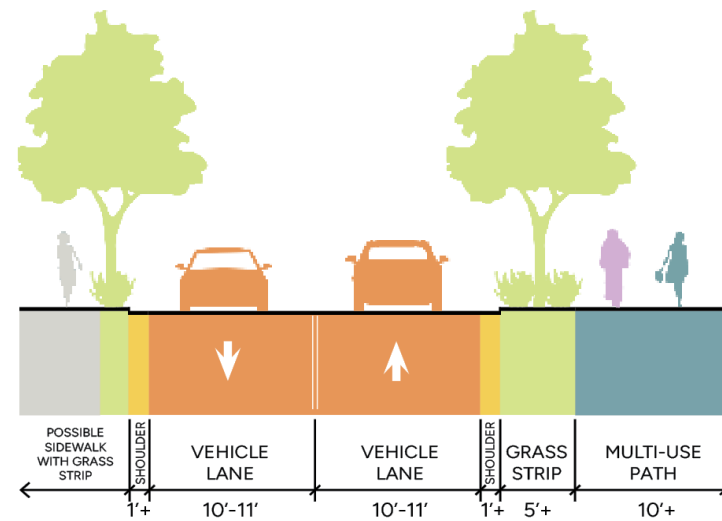


Figure 25: Multi-Use Path on One Side – Cross Section C

In situations where there is the desire, or need, to have sidewalk and bicycles on both sides of a road, a multi-use path on both sides can be considered. **Figure 26** shows the typical cross-section for that treatment. This cross-section is referenced throughout the text, conceptual recommendation graphics, and Implementation Matrix as “Cross-Section D.”

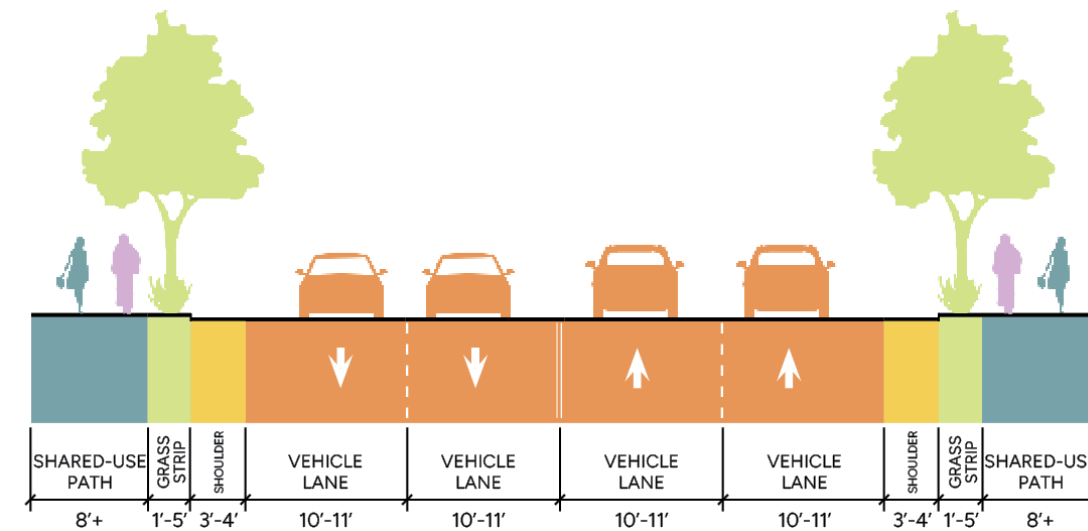


Figure 26: Multi-Use Path on Both Sides – Cross Section D

3.2.4 Courtesy Signage

Bike-themed courtesy signage reminds motorists that bicyclists might occupy the travel lane. Per the Manual on Uniform Traffic Control Devices (MUTCD), these signs may be used on roadways where no bicycle lanes or adjacent shoulders usable by bicyclists are present and where travel lanes are too narrow for bicyclists and motor vehicles to operate side by side. Throughout Avon, signage is intended to increase awareness and knowledge among motorists that bicyclists are present.



Source: MUTCD R4-2



Source: MUTCD W11-1

3.3 General Recommendations (All-Users)

3.3.1 Fog Lines and Paved Outside Shoulder

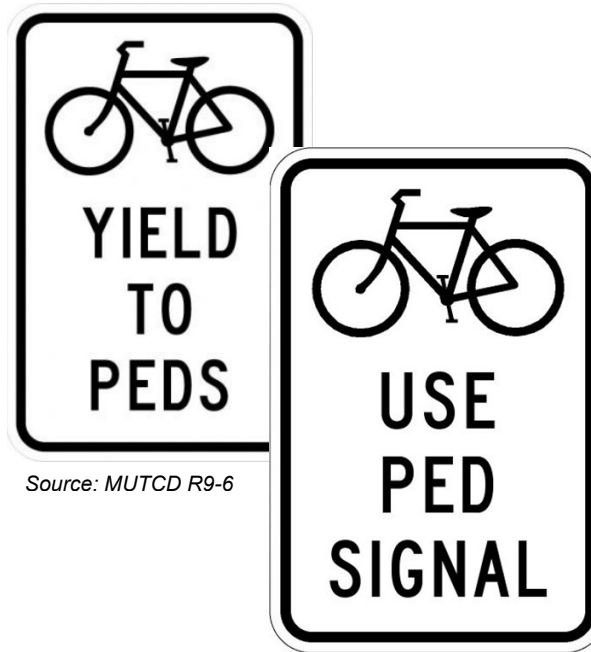
There are some roads in Avon where having a separate facility for bikes or pedestrians is infeasible, due to high cost or lack of available right-of-way, particularly on low-volume and low-speed local, residential roads. On these roads, fog lines can be painted to visually narrow the travel lanes and provide a paved outside shoulder for those pedestrians or cyclists who are utilizing the roads already. It is noted that paved shoulders are not a pedestrian facility and not expected to induce an increase in pedestrian or bike activity but are instead intended to provide some visual separation from vehicles for those pedestrians already walking or biking on the roads.



3.3.2 Signal Signage

There are circumstances where signage may be appropriate to minimize conflicts between pedestrians and bicyclists. Sometimes circumstances come into confluence on certain roadways. These factors are:

- 1 Bicyclists are allowed on sidewalks
- 2 The roadway is heavily travelled, and vehicular speeds are high, making on-road, unseparated facilities unsafe; and
- 3 There are strong attractions to a location along a roadway
- 4 In cases where these criteria are met, some courtesy reminders of the co-existence of pedestrians and bicyclists is desirable. Some samples of this signage, perhaps at signalized intersections with crosswalks are shown.



3.3.3 Speed Maintenance and Awareness

Pedestrians and cyclists are particularly vulnerable to injury in collisions with vehicles, particularly at high speeds. Traffic calming and speed management are important measures in increasing safety for all users. Radar speed feedback signs, which display both the posted speed limit and a targeted message such as "YOUR SPEED XX" or "SLOW DOWN", are designed to display feedback to those drivers who exceed the speed limit or predetermined speed threshold.

Additionally, narrowing vehicle travel lanes can help promote slower driving speeds which, in turn, reduces the severity of crashes. Reducing lane widths from 12 feet to 10 feet or 11 feet, where appropriate, can have a positive impact on safety without affecting traffic operations. The allowed vehicle lane widths for roads throughout Avon are summarized in **Chapter 2.3** Annual Pavement Plan, Table 4 and are based on the CTDOT *Highway Design Manual*.



4.0 Recommendations

Infrastructure recommendations were developed based on feedback from the community, right-of-way and other physical constraints, and the locations of important destinations around which the walksheds were mapped. Policy, education, and program recommendations were also made based on public feedback, and on best practices in transportation planning.

4.1 Policy and Program Recommendations

4.1.1 Farmington Canal Heritage Trail Speed Limit

The speed limit on the Farmington Heritage Canal Trail is 12 mph, but trail users may not be aware of this. Painting the speed limit at intervals on the path is a way to create awareness and govern speed – especially of e-bike and e-scooter riders.

4.1.2 Zoning Updates

The Town of Avon is currently updating its Zoning Regulations and among its considerations are to:

- Review the requirements for developers to install sidewalk in certain zoning districts.
- Require bicycle parking as a condition for new development, or major renovations

4.1.3 Education & Program Recommendations

Continue to support the work of Bike Walk Avon – which is responsible for much of the town’s current education and programming such as:

- Bike rodeos, trail clean up days, sponsored group rides and hikes, bike to work and school days, and used bicycle collections offered throughout the year.
- A Bicycle Education program for fourth graders attending the Avon Public Schools.

4.2 Pedestrian and Bicycle Infrastructure Recommendations

Figures graphically depicting the pedestrian and bicycle facility improvements are shown in **Appendix C**. For pedestrian recommendations, the town was segregated into three walksheds. Each of the three walksheds were further divided into smaller areas at a scale appropriate to illustrate the conceptual recommendations. For the bicycle recommendations, the Town was segmented into three main areas at an appropriate illustrative scale.

The following discussion provides an overview of the recommended improvements, listed alphabetically by street. The names of the corresponding referenced maps are included for each street—these maps provide the graphic portrait of the improvements. For example, the recommendations for Arch Road can be seen graphically on Walkshed Map Section 4, page C5 in **Appendix C**.

The plan for implementing and prioritizing these recommendations is described in **Chapter 5**. An Implementation Matrix has also been provided and can be found in **Appendix D**. The information provided in the Implementation Matrix has been provided in spreadsheet format to the Town of Avon so that it can be sorted based on project location, cost, time frame, or safety/connectivity concern.

4.2.1 Arch Road

Pedestrian (Walkshed Map Section 4 – C5)

Sidewalks should be added on Arch Road between Old Farms Road and Security Drive to connect the existing sidewalks on Old Farms Road—which provide access to Old Avon Village – and to the Farmington Canal Heritage Trail. Crosswalks are already present on Arch Road at the intersection with Security Drive across the east and west legs of the intersection, but the connectivity at this intersection is incomplete, as the both crosswalks leads directly to grass (no sidewalk or landing area). Additional crosswalks should be added on the north and south legs of the intersection to connect all existing segments of sidewalk, and ADA-compliant ramps should be added to all corners of the intersection.

Bicycle (Bike Recommendations NE Section – C19)

In the short term, courtesy signage should be added on Arch Road for eastbound traffic at West Avon Road, for westbound traffic at Old Farms Road, and in both directions near Haber Drive. In the long term, buffered bike lanes or a two-way multi-use path should be added to accommodate bicycles. The addition of a multi-use path would also allow pedestrians to be accommodated with cyclists.



4.2.2 Chevas Road

Bicycle (Bike Recommendations WEST Section – C21)

On Chevas Road, fog lines should be striped in the short term; based on the CTDOT *Highway Design Manual*, the minimum allowed vehicle lane width for Chevas Road is 11 feet. Courtesy signage at either end is recommended as well. In the long term, bike lanes should be added, although there are significant constraints that might complicate the addition of bike lanes on Chevas Road.

4.2.3 Climax Heights Road

Pedestrian (Walkshed Map Section 4 – C5)

Climax Heights Road is the access road for several town public buildings, such as Town Hall, the police department, and the Recreation and Parks Department, and is also part of the Farmington Canal Heritage Trail. Additional sidewalks should be added to connect the trail west of the town buildings to the trail along Ensign Drive, since no pedestrian facilities currently exist along this stretch. The corner radius at the southeast corner of the intersection of Climax Heights Road and Ensign Drive should also be reduced, which will force turning vehicles to slow down and shorten the crossing distance for pedestrians and bicycles crossing Ensign Drive. Lastly, additional Farmington Canal wayfinding signage should be added along Climax Heights Road, which will also benefit cyclists.

4.2.4 Country Club Road

Pedestrian (Walkshed Map Sections 7, 8, and 14 – C9, C10, C17)

On Country Club Road, sidewalks should be added on the north side of the road between Lovely Street and Carriage Drive. A crosswalk with RRFBs should be installed across Country Club Road near Carriage



Drive, along with pedestrian ahead warning signage in advance of the proposed crossing. The new sidewalks and crossings will connect the Roaring Brook School to the neighborhoods to the south.

The existing sidewalks on Country Club Road between West Avon Road and Sunnybrook Drive were in poor condition, but were upgraded in the summer of 2023. The town has been tentatively awarded a LOTCIP grant to fund the installation of a pedestrian facility on Country Club Road between the public library and Old Farms Road. A sidewalk or a multi-use path should be installed on Country Club Road. New crosswalks and ramps should be added on Avonside, Stony Corners Road, Burnham Road, and Winding Lane.



Figure 27: Potential Intersection Treatment

Source: <https://www.ennisflint.com/products/trafficpatternsxd>

Country Club Road is also a potential candidate for an intersection treatment or decorative crosswalks at the intersections of Sunnybrook Drive, the public library main driveway, and Burnham Road.

Bicycle (Bike Recommendations, WEST, NE, and SE Sections – C19, 20, and 21)

In the short term, courtesy signage should be added on Country Club Road at several locations. Radar speed feedback signs are recommended in three strategic locations between Lovely Street, West Avon Road, and Old Farms Road. In the long term, buffered bike lanes or a two-way multi-use path should be added to accommodate bicycles. The addition of a multi-use path would also allow pedestrians to be accommodated with cyclists.

4.2.5 Hollister Drive

Bicycle (Bike Recommendations SE Section – C20)

Courtesy signage should be added on Hollister Drive near Crestwood Drive and West Avon Road in the short term. In the long term, buffered bike lanes or a two-way multi-use path is recommended to accommodate bicycles. The addition of a multi-use path would also accommodate pedestrians.

4.2.6 Huckleberry Hill Road

Pedestrian (Walkshed Map Sections 12 and 13 – C15 and C16)

Huckleberry Hill Road provides access to several recreational areas within the town, including Countryside Park, Buckingham Park, and the Huckleberry Hill trail system. Sidewalks should be installed on the west side of Huckleberry Hill Road between Verville Road and Deer Run, and on the east side of the road

between Buttonwood Hill Road and Edwards Road to facilitate connectivity between the nearby residential areas and the recreational facilities. Sidewalk is not recommended between Edwards Road and Deer Run, as there are significant grading and right-of-way challenges on this section of Huckleberry Hill Road. Pedestrians can still access all recreation areas without this sidewalk via the connection between Edwards Road and Buckingham Road behind Countryside Park, although this route is more circuitous.

Between Buckingham Road and Deer Run, the bridge over Hawley Brook limits the installation of sidewalk; next to the guiderail at this location there is an existing unimproved path that is worn by regular pedestrian use, which should be formalized and widened to a four-foot path. The guiderail should also be replaced and upgraded to better protect pedestrians from passing vehicles.

There are two areas on Huckleberry Hill Road where existing stone walls near the edge of the roadway also prevent the installation of sidewalks. In these sections, Huckleberry Hill Road should be realigned and shifted east to avoid the stone wall obstructions, which has the twofold effect of creating enough buffer to add sidewalk in front of the stone walls and creating a slight deflection (chicane) in the road to decrease motorist travel speeds.

Crosswalks should be added across Huckleberry Hill Road at Deer Run and Edwards Road, accompanied by RRFBs, to facilitate pedestrians crossing to recreation areas on the other side of the road from the proposed sidewalks.

Bicycle (Bike Recommendations WEST Section – C21)

In the short term, courtesy signage should be added on Huckleberry Hill Road near the town line with Canton and near the intersections with Verville Road, Buckingham Road, Chevas Road, and New Road. Radar speed feedback signs should also be installed strategically along the entire length. They are shown conceptually at the town border with Canton and near Verville Road, Edwards Road, and New Road. In the long term, bike lanes should be added on Huckleberry Hill Road, although there are significant constraints that would need to be overcome.

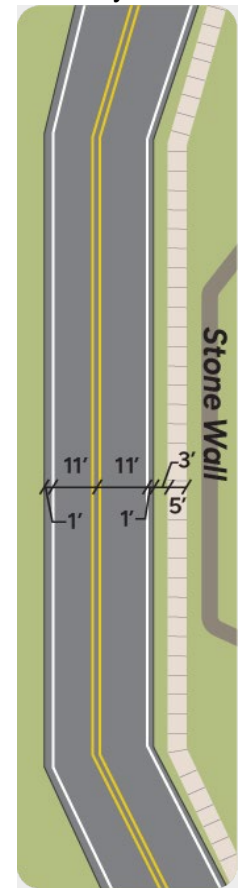


Figure 28: Conceptual Road Shift

4.2.7 Juniper Drive

Bicycle (Bike Recommendations WEST Section – C21)

In the short term, fog lines should be striped on Juniper Drive. Based on the CTDOT *Highway Design Manual*, the minimum allowed vehicle lane width for Juniper Drive 10 feet. In the long term, bike lanes should be added.

4.2.8 Lovely Street (CT-177)

Pedestrian (Walkshed Map Section 14 – C17)

Sidewalks should be installed on Lovely Street between Northgate and Blueberry Lane. The addition of these sidewalks will connect Roaring Brook School to the surrounding neighborhoods to the north, west, and east, as pedestrians can use the lower-volume neighborhood roads to make their way to Northgate



and Blueberry Lane, then to the proposed sidewalks on Lovely Street and Old Wheeler Lane to the school. A crosswalk with RRFBs should be added across Lovely Street at the intersection with Northgate; pedestrian ahead warning signs should also be installed. A pedestrian bridge will need to be constructed across Roaring Brook to accommodate the proposed sidewalk.

Bicycle (Bike Recommendations WEST Section – C21)

On Lovely Street, courtesy signage should be added in the short term. The CTDOT VIP Paving Program should be monitored with the intention of eventually narrowing vehicle lanes and increasing space for non-motorists. The town should also contact CTDOT about possible installation of radar speed feedback signs. In the long term, buffered bike lanes or a two-way multi-use path should be added. A multi-use path would also be able to accommodate pedestrians.

4.2.9 Mountain View Avenue

Pedestrian (Walkshed Map Sections 2 and 3 – C3 and C4)

Sidewalks should be constructed on Mountain View Avenue to connect the Route 44 pedestrian network to the existing sidewalks on Mountain View Avenue, which are present between Manion Lane and Eddy Street. These sidewalks allow users to access Sperry Park and the Farmington Canal Heritage Trail.

Bicycle (Bike Recommendations NE Section – C19)

In the long term, the existing and proposed sidewalk should be widened to a multi-use path to accommodate cyclists.

4.2.10 Nod Road/Waterville Road (CT-10)

Pedestrian (Walkshed Map Section 1 – C2)

The pedestrian recommendations on Nod Road include adding sidewalk on the east side of the road from the Hunter's Run neighborhood/subdivision to Route 44 and on the west side of the road from Route 44 south to the Alsop Meadows trailhead on Waterville Road (CT-10).

Sidewalks or a formalized trail should be added on Waterville Road between Old Farms Road and Bishop Lane. The signal at Old Farms Road should be upgraded to allow pedestrians to cross at the north leg of the intersection, including installing a crosswalk and adding pedestrian signal heads and push buttons. The addition of the sidewalk and crossing will help facilitate a connection between the neighborhoods to the east of Waterville Road and the Fisher Meadows Recreation Area. Eventually, the town should investigate adding a trail between Deercliff Road and Bishop Lane to allow those residents on Deercliff Road to more easily access the recreation areas.

Bicycle (Bike Recommendations NE and SE Sections – C19 and C20)

On Waterville Road, courtesy signage should be added in the short term. The CTDOT VIP Paving Program should be monitored with the intention of eventually narrowing vehicle lanes and increasing space for non-motorists. The town should also contact CTDOT about possible installation of radar speed feedback signs. In the long term, buffered bike lanes or a two-way multi-use path should be added on Waterville Road.

4.2.11 Old Farms Road

Pedestrian (Walkshed Map Section 8 – C10)

A crosswalk with RRFBs should be added on Old Farms Road across the north leg of the intersection with Arch Road. Pedestrian ahead warning signs should be added in advance of this crossing.

At the intersection with Country Club Road, a new crosswalk with RRFBs and "Pedestrian Ahead" warning signage should be installed to connect the proposed pedestrian facility on Country Club Road to the northern Fisher Meadows trailhead.

at the northwest corner of Old Farms Road and Waterville Road (CT-10), the existing sidewalks should be extended some 100 feet from the seating area to the new crosswalk proposed across Waterville Road (CT-10).

Pedestrian (Walkshed Map Sections 14 – C17)

Sidewalks should be installed on Old Wheeler Lane to enable pedestrians to walk to Roaring Brook School. Crosswalks should be added across Old Wheeler Court, the Roaring Brook school driveway, and Camelot Lane. Crosswalks should be added across Old Wheeler Lane at the intersections with Lovely Street and Country Club Road.

4.2.12 Route 44 (West Main Street/East Main Street)

Pedestrian (Walkshed Map Sections 1 through 6 – C2 through C7)

In general, sidewalks should be added on Route 44 on both sides of the road when feasible. New crosswalks are shown across Route 44 to enhance pedestrian connectivity between both sides of the road, and across intersecting roads or major driveways. Upgrades to pedestrian signals are also proposed as needed, which includes installing pedestrian signal heads and push buttons and updating from existing side street green phasing. ADA-compliant landing areas at each of the existing bus stops on Route 44 are recommended. Sidewalk signage showing pedestrian priority should also be added strategically throughout the Route 44 corridor.

There are areas along Route 44 that are constrained by grade, wetlands, or available width; these constraints prevent sidewalks from being installed on both sides of the road or require additional infrastructure improvements to accommodate the proposed pedestrian facilities. These areas are discussed in more detail below.

In lieu of a traditional sidewalk, a protected pedestrian facility is proposed across the Farmington River Bridge, which will include an on-road sidewalk and a protective barrier to separate pedestrians from passing vehicles. Route 44 should be restriped in this area to narrow the travel lanes and shift the centerline slightly south to accommodate the new pedestrian facility. If the road is crowned, shifting the striping will result in an adverse driving condition; the road should be repaved as necessary to accommodate the new travel lanes created by the restriping.



Near Old Avon Village, just west of the Farmington River bridge, sidewalks are proposed on both sides of the road. Due to existing grades, retaining walls will likely need to be built to accommodate sections of the new sidewalk, such as in front of Old Avon Village on the south side of Route 44 and just west of Mountain View Avenue on the north side of Route 44. Also in that area, a trail connection and new wayfinding signage are recommended between Old Avon Village and the gas station, connecting to the existing River Park trails in that area.

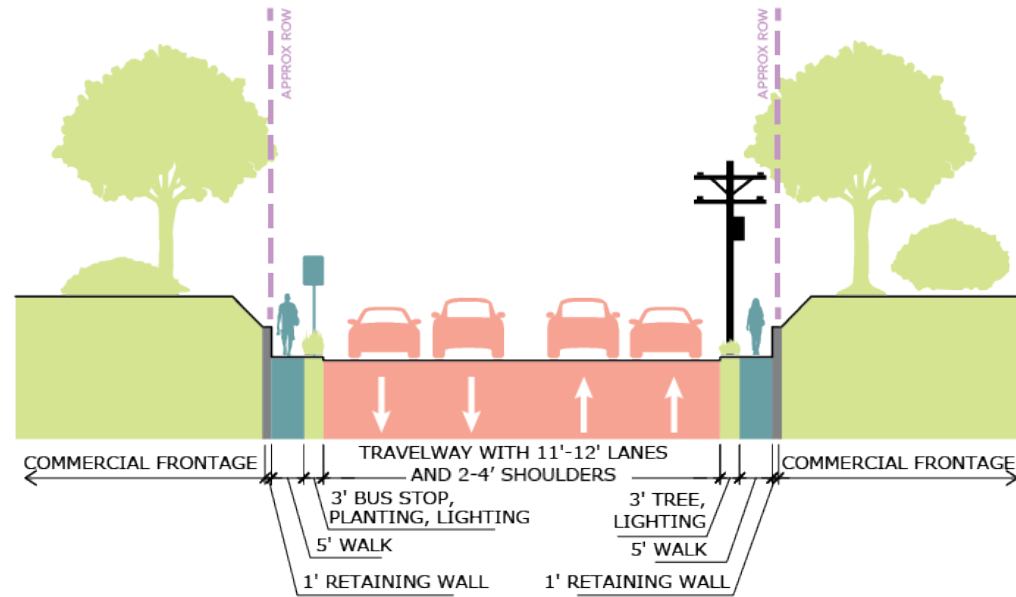


Figure 29: Cross-section with Retaining Wall

Note: For segments of Route 44 near Old Avon Village

Near Whole Foods, the existing sidewalk at the signalized intersection of Route 44 and Climax Road should be extended along the Whole Foods frontage and a new stair connection or ADA-grade path should be constructed to connect the new sidewalk with the Farmington Canal Heritage Trail. Due to the proximity of Stub Pond to the roadway, sidewalks should be constructed on the north side of Route 44 between Climax Road and the Avon Colonial Manor access driveway to minimize the potential impact to the wetlands in that area.

West of the Walmart on Route 44, the bridge across Nod Brook limits the installation of sidewalks. In this area, a pedestrian bridge should be constructed across Nod Brook on the south side of the road, which will likely require an easement from the adjacent property owner.

Bicycle (Bike Recommendations NE Section – C19)

The recommendations for accommodating bicycles on Route 44 are largely long-term. In the short term, signage showing pedestrian priority should be added strategically throughout the Route 44 corridor.

In the long term, the future and existing sidewalks should be widened to multi-use paths. Between Dale Road and Plaza 44, a rear service road should be constructed behind the shopping plazas, or the future sidewalk should be widened to a shared-use path.

4.2.13 Scoville Road

Pedestrian (Walkshed Map Sections 10 and 11 – C12 and C13)

The existing sidewalk between West Avon Road and the Farmington Canal Heritage Trail entrance should be widened to a multi-use path to accommodate both bicycles and pedestrians.

4.2.14 Simsbury Road

Pedestrian (Walkshed Map Section 3 – C4)

Just north of the intersection with Route 44, there is an existing midblock crossing with warning signage across Simsbury Road. While only one southbound lane is striped, there is available width for two vehicles driving side-by-side to approach the crosswalk, creating a potential multiple-threat for pedestrians. A multiple-threat situation involves a motorist in one lane stopping for a pedestrian while a motorist in another lane may not. To minimize the potential for a multiple-threat collision, a pedestrian island should be installed, and the southbound lane should be reduced in width to only allow one vehicle to approach the crosswalk at a time. To enhance crosswalk visibility, RRFBs should be added at this location.

4.2.15 Thompson Road

Pedestrian (Walkshed Map Sections 10 and 11 – C12 and C13)

New sidewalk should be added on Thompson Road between West Avon Road and the Farmington Canal Heritage Trail, past the Thompson Brook School. New crosswalks and ramps should be installed at the Thompson Brook School driveways. To accommodate the new sidewalk, a pedestrian bridge will need to be constructed across Thompson Brook.

4.2.16 Waterville Road

See Nod Road/Waterville Road (CT-10) write up.

4.2.17 West Avon Road (CT-167)

Pedestrian (Walkshed Map Sections 7, 9, and 10 – C9, C11, and C12)

West Avon Road provides access to both the middle school and the high school. There are sidewalks already present on West Avon Road between Avon Middle School and Scoville Road, but sections of these sidewalks are in poor condition and should be widened to five feet. The town has started the project to upgrade these existing sidewalks. New sidewalk should be installed between Scoville Road and Thompson Road to further connect to the Thompson Book School. Crosswalks and ramps should be added across Sudbury Way, Day Road, and Scoville Road. A crosswalk should also be added across West Avon Road at the intersection with Scoville Road to provide pedestrian access to the Avon Senior Center. The signal at Scoville Road will also need to be upgraded to provide adequate pedestrian crossing conditions, including adding pedestrian signal heads and push buttons.

Bicycle (Bike Recommendations NE and SE Sections – C19 and C20)

On West Avon Road, courtesy signage should be added in the short term. The CTDOT VIP Paving Program should be monitored with the intention of eventually narrowing vehicle lanes and increasing space



for non-motorists. The town should also contact CTDOT about possible installation of radar speed feedback signs. In the long term, buffered bike lanes or a two-way multi-use path should be added on West Avon Road.

4.2.18 Woodmont Road

Bicycle (Bike Recommendations NE Section – C19)

Courtesy signage and radar speed feedback signs should be added on Woodmont Road near the intersection with Stagecoach Road.



5.0 Implementation Plan

This study has resulted in over a hundred recommendations throughout the Town of Avon. To help prioritize and facilitate progress in implementing them, an Implementation Matrix has been developed to help guide decision-making; this Matrix is included in **Appendix D**. Each individual improvement (as described in **Chapter 4** and shown graphically in **Appendix C**) is listed and sorted alphabetically by street. Additional elements have been added to each recommendation, including a cost estimate, timeframe, and identification of any potential connectivity or safety concerns keyed to each recommendation. The Matrix has also been provided to the Town of Avon staff in spreadsheet form so all recommendations can be sorted or filtered by criteria as needed. There are also several public comments in **Appendix B** that may guide the town’s decisions.

The cost parameter is shown as \$, \$\$, or \$\$\$, and relates generally to the following:

- \$ - Less than \$100,000
- \$\$ - Between \$100,000 and \$300,000
- \$\$\$ - Greater than \$300,000

In some cases, the cost to implement a recommendation is only a matter of time and materials (\$). Higher costs could be related to design and permitting or require some moderate construction costs (\$\$). The highest cost elements may have specific constraints to overcome, such as removal or relocation of existing structures, ROW, or design components (\$\$\$). The \$\$ and \$\$\$ categories likely would require the town to seek funding support outside their own coffers. There are several opportunities available for transportation infrastructure in Avon that were shown in the presentation at the third public meeting. They include \$4,000,000 of funds that have not yet been committed for the Country Club Road project, and funds already spent on sidewalk improvements on West Avon Road. This demonstrates the town’s ongoing efforts to address the pedestrian and bicycle infrastructure throughout Avon.

Table 6: Funding Sources for Transportation Infrastructure in Avon (2023)

Project / Infrastructure Improvement Name	Town Budget	Grant
Capital Improvement Plan (road improvements)	\$419,791	
Council-approved supplemental appropriation for road improvements	\$750,000	
Preliminary Approval of Country Club Road LOTCIP		\$4,000,000
Sidewalk Repairs	\$25,000	
✓ Replace existing Sidewalks on West Avon Road from Sunnybrook Drive to 460 West Avon Road (COMPLETED)	\$25,000	\$128,205
+ \$424,000 (Phase II – 460 West Avon Road to Scoville Road to the FCHT)		\$424,000
Trail Maintenance for walking and hiking trails (in the recreation and parks budget)	\$15,000	
Old Farms Road Community Connectivity Grant – Phase I: Install a multi-use trail adjacent to the new North/South section (construction began Fall 2023)		\$350,000
Old Farms Road Community Connectivity Grant – Phase II (not awarded in 2023 but will be considered in the next funding cycle.)		\$800,000
Total	\$1,234,791	

The recommendations are also categorized as either short-term or long-term. For this report, short-term is defined as a project that would not be slowed by permitting, design, and/or cost of overcoming existing physical constraints. These could typically be initiated and implemented within one to two years. The longer-term projects would include one or more of the constraints listed above.

Lastly, some recommendations address specific safety or connectivity concerns throughout the town. It is suggested that these recommendations be prioritized as possible, as they will either assist in improving the safety for bicycles and pedestrians or will fill key connectivity gaps between existing town facilities.

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6.0 Resources

Technical Resource	Link
Federal Highway Administration. (2009). <i>Manual on Uniform Traffic Control Devices</i> . Washington, D.C.	https://mutcd.fhwa.dot.gov/index.htm
Connecticut Department of Transportation. (January 2023 revision). <i>Highway Design Manual</i> .	https://portal.ct.gov/-/media/DOT/documents/AEC/Manuals/Highway-Design-Manual_2023-01_v2.pdf
Connecticut Department of Transportation. (2023). Complete Streets Controlling Design Criteria and Justification Process.	https://portal.ct.gov/-/media/DOT/documents/AEC/ECD-2023-8 Complete Streets Controlling Design Criteria final sah.pdf
Capitol Region Council of Governments (2023). <i>Regional Transportation Safety Plan</i> .	https://crcog.org/wp-content/uploads/2023/06/RTSP2023.pdf



Appendix A Existing Conditions

Bicycle and Pedestrian Master Plan

Town of Avon

SLR Project No.: 141.13130.00016

February 29, 2024



U.S. Route 44

From Simsbury Town Line to Nod Road



- Roadway Characteristics
 - 10-12' lanes with 1-10' shoulders
 - Right-of-Way (ROW) varies 62' to 100'+
- Annual Average Daily Traffic (AADT) = 17,000 – 26,000 vehicles
- Speed limit of 35 mph (with travel speeds much higher)
- Identified constraints:
 - Bridge (Farmington River)
 - Culverts & Wetlands (Nod Brook, Stub Pond)
 - Lot of fast traffic
 - Intermittent sidewalks
 - Not many crossings



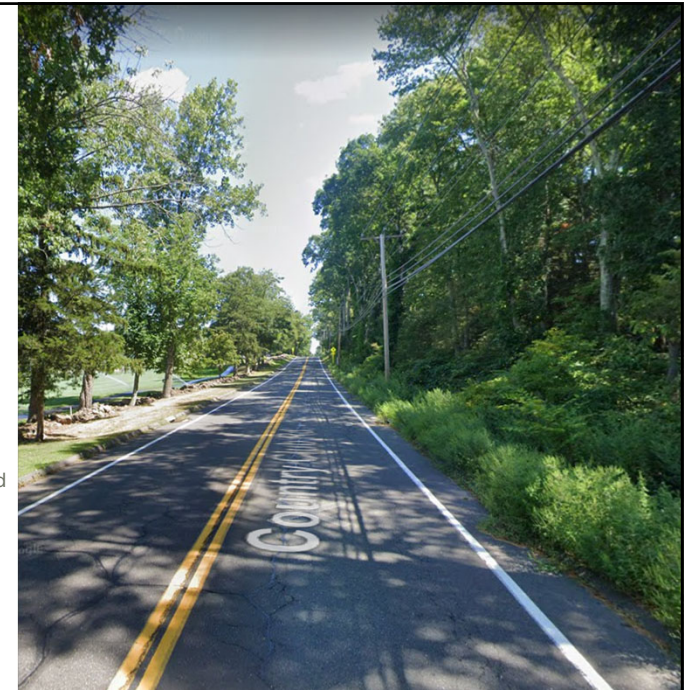
Route 44 / 202 near Walmart & Big Y
Image from Google Street View

Country Club Road

From Lovely Street to Old Farms Road



- Roadway Characteristics
 - 11-12' lanes, 2-5' shoulders
 - 50' of available right-of-way (ROW)
- Annual Average Daily Traffic (AADT) = 7,100 to 8,600 vehicles
- Speed limit of 30 to 35 mph
- 85th percentile speeds: 49.8 to 56.8 mph
- Identified constraints:
 - Bridge over Roaring Brook
 - Guide rail between Old Wheeler Lane and Brookmoor Road
 - Stone wall near Tanglewood Drive
 - Bridge over Big Brook
 - Bridge over Chidsey Brook
 - Stone wall adjacent to road near golf course
 - Steep grade and winding east of Winding Lane
- Tentatively awarded grant to build new sidewalk



Country Club Road between Burnham Road and Tamara Circle
Image from Google Street View

Burnham Road

From West Avon Road to Scoville Road



- Roadway Characteristics
 - 10-14' lanes, 2-6' shoulders
 - 50' of available right-of-way (ROW)
- AADT and travel speed data unavailable
- Speed limit of 30 mph
- Identified constraints:
 - Cemetery at intersection with Country Club Road
 - Dense mature tree growth close to road
- Street is being repaved this year



Burnham Road between Foxcroft Run and Sleepy Hollow Road
Image from Google Street View

Juniper Drive

From Lovely Street to Stagecoach Road



- Roadway Characteristics
 - 12-13' lanes, no shoulders
 - 60' of available right-of-way (ROW)
- Annual Average Daily Traffic (AADT) = 1,700 vehicles
- Speed limit of 25 mph
- 85th percentile speed: 45.2 mph (west of Stagecoach Road)
- Identified constraints:
 - Horizontal curvature limits visibility
 - Sections of steep grade on road
- Street is being repaved this year



Juniper Drive between Avonridge and Stagecoach Road
Image from Google Street View



Arch Road

From West Avon Road to Old Farms Road

- Roadway Characteristics
 - 11-14' lanes, 2-6' shoulders
 - 50' of available right-of-way (ROW)
- Annual Average Daily Traffic (AADT) = 4,100 vehicles
- Speed limit of 30 mph
- 85th percentile speed: 42.7 mph (east of West Avon Road), 33.6 mph (east of Security Drive)
- Identified constraints:
 - Steep grade throughout
 - Overhead bridge near Old Farms Road
 - First Company Governor's Horse Guard near intersection with West Avon Road
 - Cemetery near intersection with Security Drive

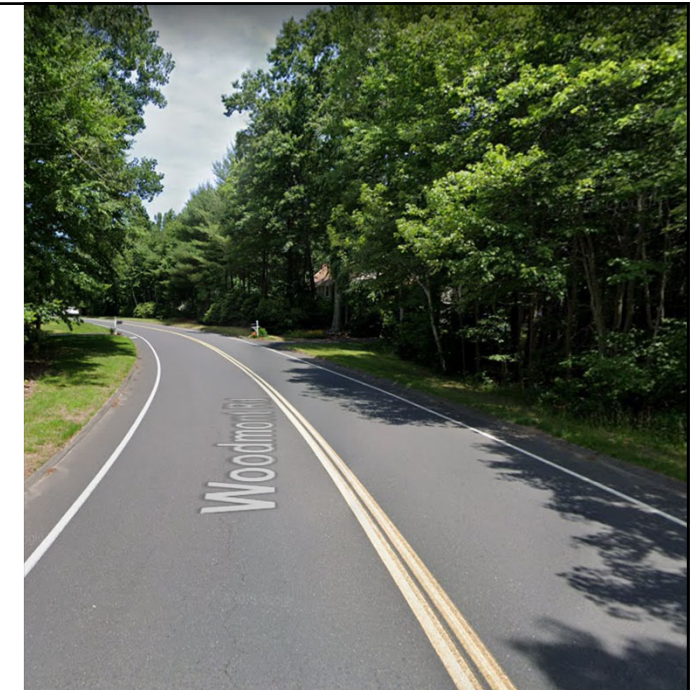


Arch Road between Richard Street and Jackson Street
Image from Google Street View

Woodmont Road

From Stagecoach Road to West Avon Road

- Roadway Characteristics
 - 12-13' lanes, 3-5' shoulders
 - 50-60' of available right-of-way (ROW)
- Annual Average Daily Traffic (AADT) = 3,000 vehicles
- Speed limit of 30 mph
- 85th percentile speed: 39.5 mph (west of West Avon Road)
- Identified constraints:
 - Guide rail on either side between Stagecoach Road and Fall Brook
 - Horizontal curvature limits visibility
 - Overhanging brush



Woodmont Road between Fall Brook and Manitook Mountain Road
Image from Google Street View

Hollister Drive

From Lovely Street to West Avon Road

- Roadway Characteristics
 - 12-13' lanes, 3-5' shoulders
 - 60' of available right-of-way (ROW)
- Annual Average Daily Traffic (AADT) = 2,900 vehicles
- Speed limit of 30 mph
- 85th percentile speed: 52.2 mph (west of Carriage Drive)
- Identified constraints:
 - Bridge over Roaring Brook & Henry's Pond
 - Bridge over Big Brook
 - Sections of steep grade on road



Hollister Drive between Lenox Road and Crestwood Drive
Image from Google Street View

Chevas Road

From Huckleberry Hill Road to Lovely Street

- Roadway Characteristics
 - 11-12' lanes, no shoulders
 - 45' of available right-of-way (ROW)
- Annual Average Daily Traffic (AADT) = 2,500 vehicles
- Speed limit of 30 mph
- 85th percentile speed: 36.8 mph (west of Lovely Street)
- Identified constraints:
 - Steep slope along north side of the road
 - Guide rail on south side of road west of Glen Hollow Drive
 - Mature trees and utility poles close to road edge on both sides



Chevas Road between Huckleberry Hill Road and Lovely Street
Image from Google Street View



Huckleberry Hill Road

From Avon Town Line to Chevas Road

- Roadway Characteristics
 - 11-12' lanes with 1-3' shoulders
 - 50' of available right-of-way (ROW)
- Annual Average Daily Traffic (AADT) = 3,000 vehicles
- Speed limit of 25 or 35 mph
- 85th percentile speed: 48.4 mph (north of Chevas Road)
- Identified constraints:
 - Bridge over Hawley Brook (narrow cross section)
 - Sections of steep grade on road
 - Guide rail between Smith Road and town line
 - Horizontal curvature limits visibility



Huckleberry Hill Road between Buckingham Road and Deer Run
Image from Google Street View

Lovely Street (CT-177)

From Dale Road to Unionville, Farmington Town line

- Roadway Characteristics
 - 11-12' lanes, 2-5' shoulders
 - 50' of available right-of-way (ROW)
- Annual Average Daily Traffic (AADT) = 7,100 to 8,600 vehicles
- Speed limit of 40 mph
- 85th percentile speeds: 49.8 to 56.8 mph
- Identified constraints:
 - Guide rail south of Mallard Drive
 - Steep slope next to shoulders



Lovely Street between Bridgewater Drive and Helena Road
Image from Google Street View

West Avon Road/Harris Road (CT-167)

From Dale Road to Farmington Town line

- Roadway Characteristics
 - 11-12' lanes, 3-6' shoulders
 - 50'-70' of available right-of-way (ROW)
- Annual Average Daily Traffic (AADT) = 5,500 to 12,900 vehicles
- Speed limit of 40 mph
- 85th percentile speeds: 44.8 to 51.7 mph
- Identified constraints:
 - Bridge over Wiggin Brook and Chidsey Brook
 - Guide rail near Avalon Drive/Dale Road, First Company Governor's Horse Guard, and Sain Ann's church
 - Fence near Meadow Ridge



West Avon Road between Burnham Road and Country Club Road
Image from Google Street View

Scoville Road

From West Avon Road to Farmington Canal trail

- Roadway Characteristics
 - 13' lanes, no shoulders
 - 50' of available right-of-way (ROW)
- No AADT or travel speed data available
- Speed limit of 30 mph
- Identified constraints:
 - Bridge over Chidsey Brook
 - Horizontal curvature limits visibility



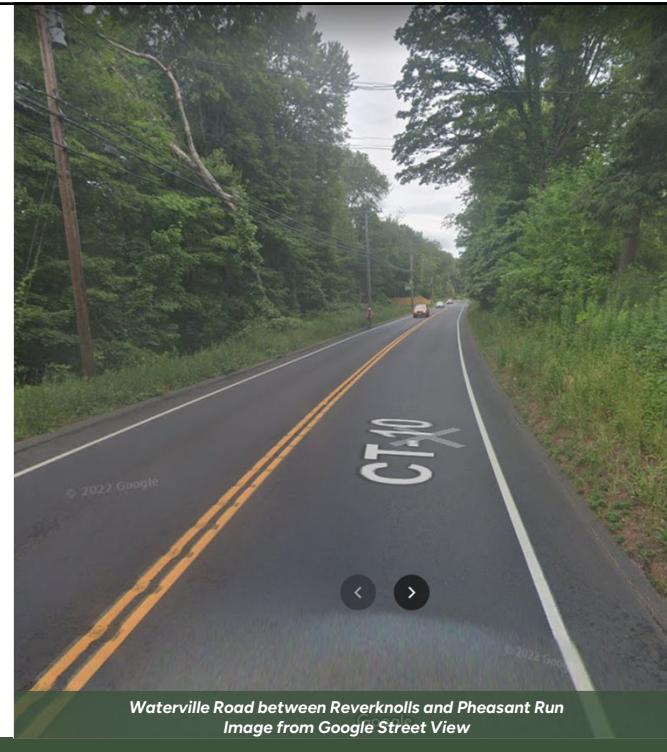
West Avon Road between Burnham Road and North Farms Road
Image from Google Street View

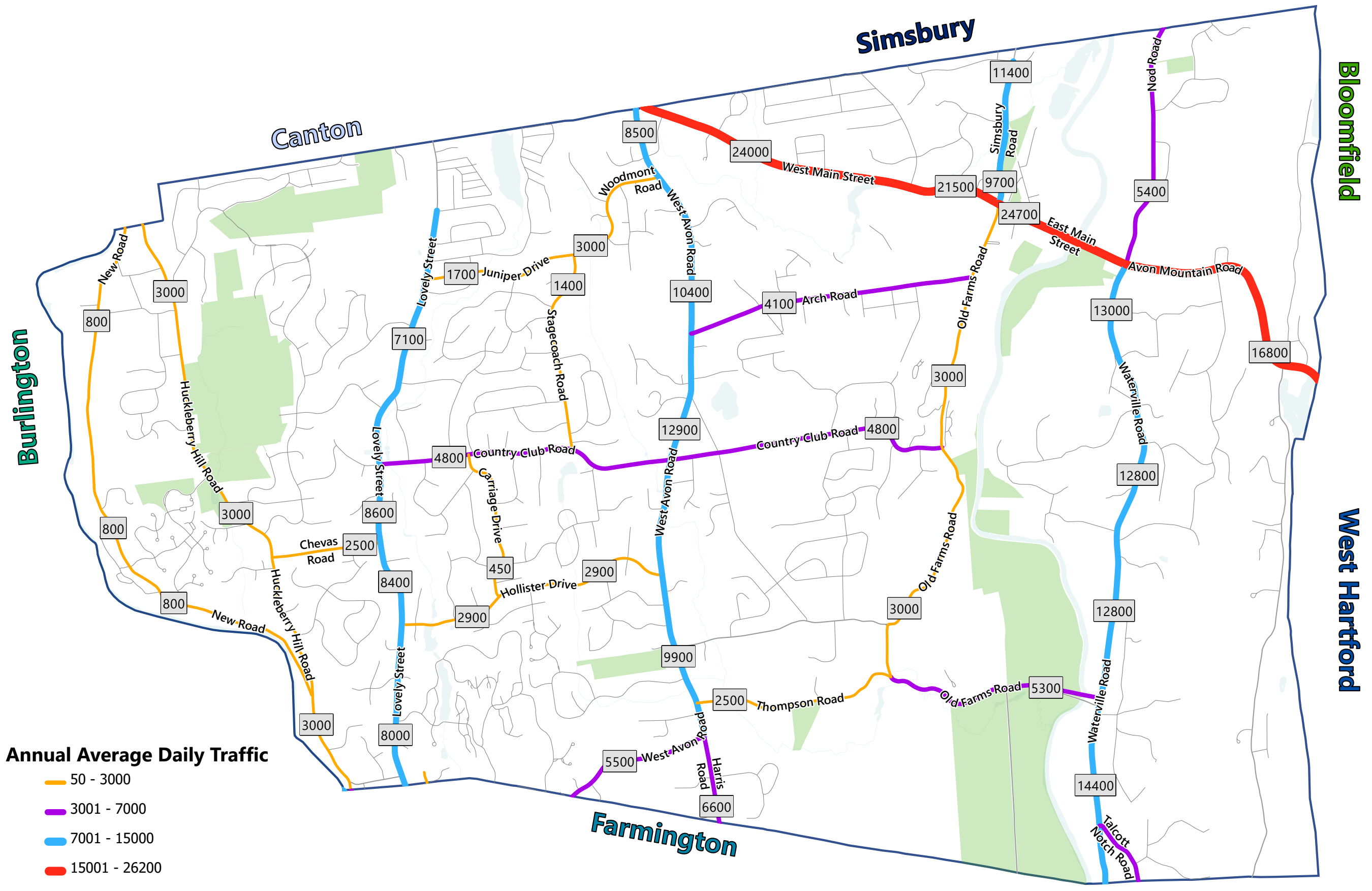


Waterville Road (CT-10)

From Old Farms Road to East Main Steet (U.S. 44)

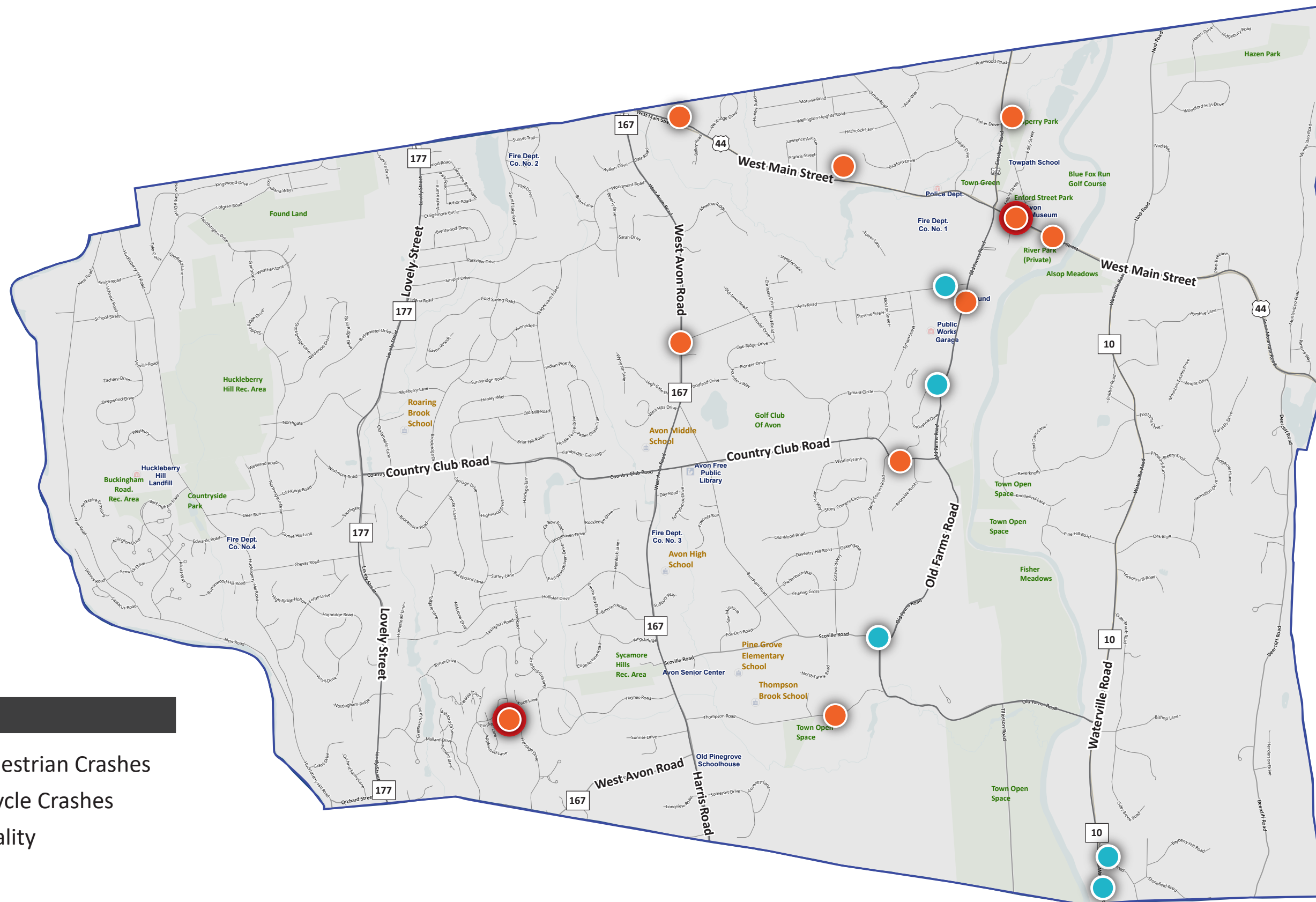
- **Roadway Characteristics**
 - 10-12' lanes, 3-5' shoulders
 - 45-60' of available right-of-way (ROW)
- **Annual Average Daily Traffic (AADT) = 12,800 to 14,400 vehicles**
- **Speed limit of 40 to 45 mph**
- **85th percentile speeds: 40.6 to 51.6 mph**
- **Identified constraints:**
 - Guide rail and fence near Old Farms Road
 - Guide rail near Cider Brook Road, Valley View Drive, and Chidsey Road
 - Bridges over brook/stream (crosses in multiple locations)





AVON ANNUAL AVERAGE DAILY TRAFFIC





Legend

- Pedestrian Crashes
- Bicycle Crashes
- Fatality

Pedestrian & bike collisions in Avon (April 2018 - April 2023)

AVON PED/BIKE CRASHES 2018-2023









**Town of Avon
Assessment of Bicycle Network Conditions**

August, 2012



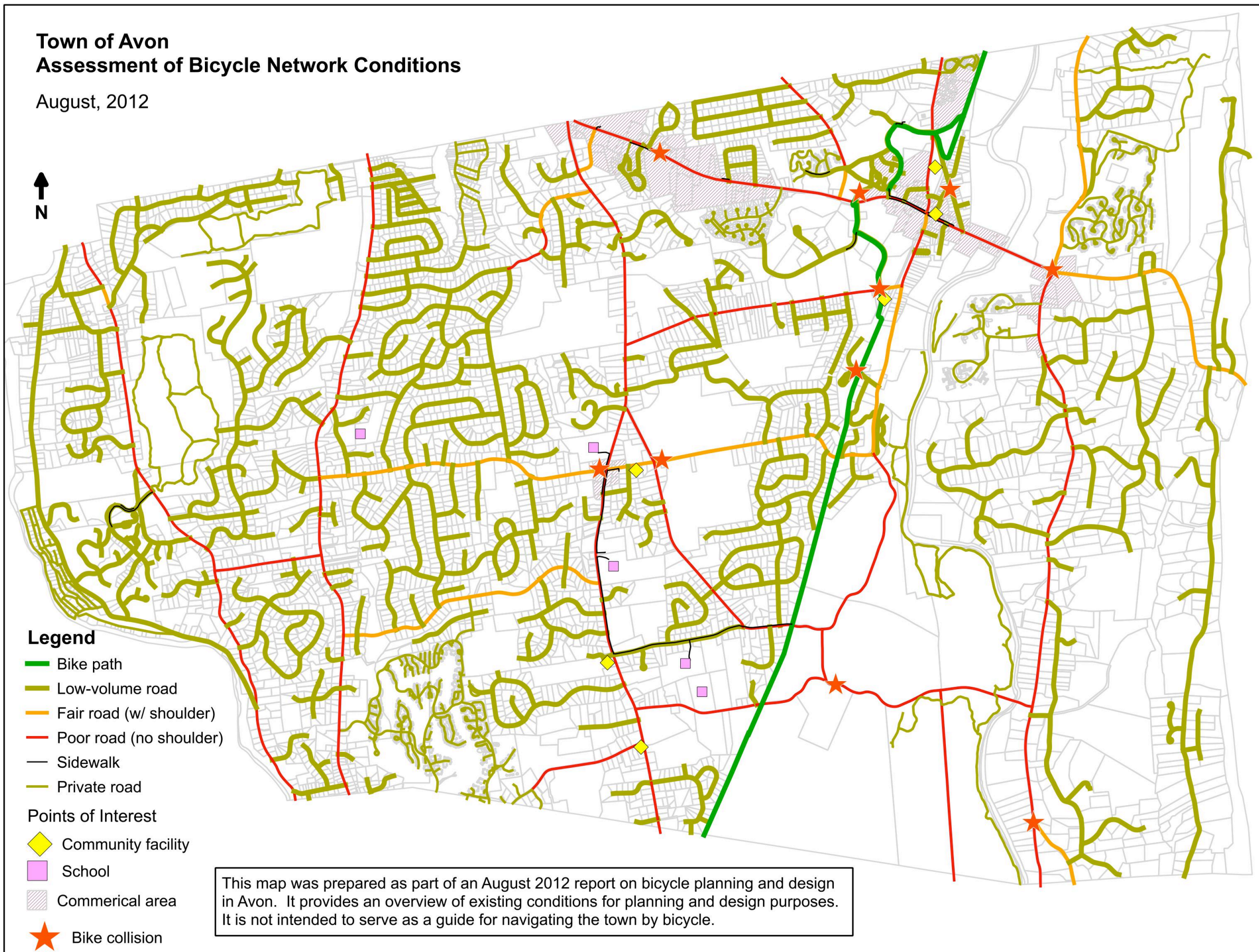
Legend

-  Bike path
-  Low-volume road
-  Fair road (w/ shoulder)
-  Poor road (no shoulder)
-  Sidewalk
-  Private road

Points of Interest

-  Community facility
-  School
-  Commercial area
-  Bike collision

This map was prepared as part of an August 2012 report on bicycle planning and design in Avon. It provides an overview of existing conditions for planning and design purposes. It is not intended to serve as a guide for navigating the town by bicycle.



Appendix B Outreach Materials

Bicycle and Pedestrian Master Plan

- Public Comments
- Meeting Notes

Town of Avon

SLR Project No.: 141.13130.00016

February 29, 2024



PUBLIC COMMENTS

Does the Vision Statement align with your vision for Avon's bicycle and pedestrian network? If not, what do you think should be included or changed?

1	Trails are wonderful recreation options but do not contribute significantly to public transportation systems. That requires linking neighborhoods with health and vital services through available and local safe walkways and alternative transportation options.
2	Eliminate reference to 'public transportation' which is a substantially separate topic with its own complex concerns and challenges.
3	Sentence #1: improved mobility for who? Seems like "bicyclists" and "pedestrians" is missing from the Vision statement. Also, connectivity should not be limited to "existing trails." I would like to see the vision include expanding the bike-ped transportation network of roads and trails.
4	It seems to put the transportation network at the center, rather than the resident/person/bicyclist/pedestrian and their needs Elements of this are spot on - mobility, health, sustainability, connectivity. It should include access and use and preservation and options and safety. And it should have citizens, residents, people at the focal point.
5	There aren't enough sidewalks and those that exist are improperly maintained to facilitate movement, there aren't assigned bike lanes on any roads, there isn't signage to warn vehicles of pedestrian/bike traffic.
6	Walkability starts outside ones front door. Having to drive to get to a trail or an area with sidewalks doesn't help much. Walkers shouldn't have to compete with cyclists, either.
7	The first thing Avon needs to do is take control of the speeding. I am off of New Rd. This road has daily walkers, joggers, bikers, dog walkers on it yet the people speeding(it is 30 miles an hour), goes up to 60. This is a daily occurrence along with the passing in no passing lanes. We need to work with police to come up with a plan. Speeders need adverse consequences to learn appropriate driving. We are human, we need consequences to learn. Folks won't be parking cars on Rt.44 to bike trails, they come through our side roads. We need to make them safer.
8	Connection to "roads and routes that connect Avon's residential areas with commercial areas, existing recreational path and trail access points, public transit, schools, and other major destinations" seems over reaching. Sure, let's connect recreation paths and schools. Leave commercial areas, public transit and other "major destinations" to the side
9	It does not talk about investing in new options only connectivity with existing trails. I'd like to see a vision that recognizes the continued importance of investments in this area, providing more sidewalks, bike paths, and safer streets.
10	The pedestrians think they're above the law and no matter what plan is implemented, until dumb people take their safety more importantly it will never be a safe environment. Bike riders and pedestrians constantly ignore the laws and rules because they're pedestrians so they have the right of way. There needs to be enforcement of laws against their bad decisions before things will improved.
11	Country Club Road needs sidewalks around Roaring Brook school. Roaring Brook school needs greater pedestrian and bicycle access.
12	Connection to "roads and routes that connect Avon's residential areas with commercial areas, existing recreational path and trail access points, public transit, schools, and other major destinations" seems over reaching. Sure, let's connect recreation paths and schools. Leave commercial areas, public transit and other "major destinations" to the side
13	As a pedestrian who walks hundreds of miles on the local trails every year and has been hit by bicyclists three times in the last two years, I am concerned about any efforts to make the the network even more bicycle friendly. The bicyclists already do not follow the rules. On a good day, maybe 25% of them alert pedestrians that they're about to pass. They do not stop at traffic signals. I follow all pedestrian rules and wear only one earbud, leaving my left ear open to listen for bicyclists, etc. Somehow most of them still sneak up on me. I'm not opposed to the vision statement in general, but I have a huge problem with seeking the bicycle friendly designation for a space that is increasingly not pedestrian friendly.
14	Any plan should recognize that bicycles present a serious hazard on many Avon roads...and bicycles should be restricted to designated bicycle paths. The dangers inherent in having bicycles on narrow heavily traveled roads with many curves and often with limited vision are exacerbated by the many bicyclists who totally ignore traffic laws.
15	ad+B1:C25d "improved and widen pedestrian bike area in existing public roads". for example, Biking on roads like country club and arch road are extremely dangerous as there is about a foot on the side of the road. and in that foot you have potholes and sewer grates. For many, these roads are necessary to travel on to get to the bike trails.

Monthly Mobility Question #1 Are there places in Avon you'd like to walk, bicycle, or "wheel" to but feel unsafe doing so?

1	Traffic speed and inadequate road surface (pavement, shoulder, lane or whatever to travel safely from our residential neighborhood to other neighborhoods, parks, schools, stores or existing rail trails/bike paths.
2	Down Lovely Street to the Farmington Town line.
3	Yes. I would love to walk to Avon Town Center and also get to the bike path to utilize it more. There aren't sidewalks that allow me to get to those locations safely.
4	Would like to bike on Country Club Road, Route 177, and West Avon Road. I have biked all three in the past to reach the bike paths and schools; however, none felt safe given the proximity and speed of passing cars and vehicles. It would be a huge improvement if there were bike lanes, or at least signs indicating these are cycling routes. In addition, I walk and run on Stagecoach Road and also Country Club Road between Stagecoach and West Avon Road. Again, vehicles go too fast and too close to runners and walkers on these roads, Country Club in particular. I see a number of middle and high school kids walking home on Country Club Road and there should be sidewalks for them.
5	west avon road west side of the street

6	West Avon Road to trails due to lack of sidewalks and crosswalks
7	West Avon road to Shopping on Rte 44 - traffic speed, lack of cycling lane, sidewalk. Ability to get from neighborhoods to the west of West Avon Rd to the rails to trails safely, same reasons as above.
8	West Avon Road and Rt 44 due to lack of sidewalk and high traffic speed.
9	West Avon Road and Rt 44 due to high speed traffic and no sidewalk
10	West Avon Rd towards Thompson Rd
11	We need sidewalks!!! I live on Hollister and would like to bike or walk on hollister especially to the high school.
12	We live in between Country Club and Hollister, both roads aren't great for biking or walking but are essential for traveling to keys areas: schools, public library, bike paths, parks, etc. It'd be great if most double yellows in that area were more catered towards biking and walking.
13	We have no true protected bike lanes except the rail trail. Physical barriers to protect bikers are the only way to make biking safer
14	We desperately need a pedestrian footpath down Hollister Drive, so kids can walk to Avon High School. It should be on the left hand side when walking down Hollister Drive towards the Avon High School and should link up to the pedestrian crossing at the end of Hollister so walkers can cross West Avon Rd at the pedestrian crossing and walk to the High School. It is NOT safe to walk down Hollister Dr at the moment as people in cars speed really badly along this road and there are hills and curves which reduce safe visibility. (From there they could also walk to the library, lukes donut shop and other shops and mailbox at those local shops and the middle school connecting to the existing footpaths that already exist).
15	We are unable to reach any of the towns bike trails or the PGS school from our home. We live on Avon mtn and rt 10 is too unsafe to bike on to access old farms road.
16	Waterville Road, Old Farms Road to Bike Trail, Route 44 /West Main Street, Waterville & Nod Road through intersection of Rt 44.
17	Waterville Road & Nod Road. But there are no sidewalks & not even adequate shoulders for safety. Plus, traffic speed & behavior. Automobile drivers frequently speed excessively AND I have been PASSED on the double yellow lines! Really! Dangerous & very scary! I live on the East side of Waterville Rd, and actually on 3 occasions over the past few years, when I was driving south, a car that was traveling a few cars behind me (and therefore, I guess, could not see my turn signal) attempted to pass the car directly behind me AND my car AT THE SAME TIME that I was slowing, signalling & beginning to turn left into my street! Terrifying! Also, I wish I could walk from Fisher Meadows to the Rails to Trails, then on to Whole Foods on Rt. 44 ... but hopefully the proposed realignment of of Old Farms Road (Thank you AOFs!), if all goes as planned, then that should be safely possible within the next few years with the accomplishment of that Town 2-phase project (long planned and envisioned!).
18	Traffic speed and width of roads for all main roads! Especially 167, 177, and Country Club.
19	To the businesses along 44: Big Y, Whole Foods, etc.
20	Thompson rd from west Avon to old farms needs a bike lane walking lane or sidewalk.
21	There should be a sidewalk on the north side Country Club Road - a road that serves two schools, the public library, a church, a golf club, a pool club, and the geographic center of town. This has been proposed to the town council and town manager many times, to no avail.
22	There are no sidewalks from Harris Road to the Avon Senior Center.
23	The west side of town has access to a Canton trail that "starts" at the intersection of Lovely and 44. The trail will take you to the Farmington River. Getting to it is difficult and dangerous. I have tried going out to Lovely and then navigate Lovely to the "trailhead" at the intersection with 44. NOT GOOD. Secondly, I have seen repeatedly the difficulty on Huckleberry for pedestrians. This has included elderly walkers with a younger family member along side attempting to stay out of the way of two trucks passing at the spot they were trying to walk. What a beautiful road that could be made into a beautiful avenue for people too. The problem is no pedestrian consideration or protection in design and build out of the roads.
24	The section of Woodmont Rd between Fall Brook and Stagecoach Rd has a fair amount of traffic. It has no shoulder or sidewalk which makes it treacherous to walk on. It connects the Bevery Dr/Sarah Dr neighborhood to Stagecoach Rd and is frequently used by the residents for walks. If it had a sidewalk or a shoulder it would be much safer and a big improvement to the neighborhood. Thank you for your consideration!
25	The Schools. Old Avon Village. The commons.
26	The first priority of the plan should be creating a safe bike lane, linking access to both bike trails. A safe biking lane rout needs to be established between the bike path entrance at Lovely Street and Rt. 44 to the bike path entrance on Country Club (or another east/west street). I understand the Lovely Street/Rt. 44 entrance is in Canton, however the majority of the bike lane on Lovely Street would be in Avon. I believe it would be a mistake to first focus on bike lanes on the western side of Country Club Road (for access to Roaring Brook School). The children who attend that school do not have the ability to ride up, or down the elevation. Please prioritize linking the bike trails with safe bike lanes.
27	Sycamore recreation area- sidewalks aren't properly maintained, speeding is an issue, have to cross a major road with faster travel speeds
28	Small shoulder on both sides of the road. I'm particularly nervous to ride here because I often see skid marks, broken glass and general remnants of car crashes. Distracted driving happens all the time on lovely street as well as speeding and crass passing by crossing the double yellow. Drivers are not thinking about bikes on this road. A bike lane from the Bike trailhead at Lovely/Rt. 44 to Hollister would be best.
29	Small shoulder on both sides of the road. Distracted driving happens all the time on Arch Road as well as speeding and cars passing by crossing the double yellow. Drivers are not thinking about bikes on this road. A bike lane from the Bike trailhead at Arch/Security to West Avon would be best.
30	Sidewalks between Lovely street and west again road would provide access to school and town facilities from multiple residential neighborhoods. As it is now it is completely unsafe to walk to school or ride to school. Speeding in residential neighborhoods particularly Country Club, Arch road, Scoville rd and many other "cross rds" is pervasive and scary.

31	RT10 from Talcott Notch Road to Fisher Meadows, traffic speed and narrowing road over brook. Would like a walkway/biketrail for this segment of the road, possibly allow pedestrian and bikes on a reconnected Cider Brook Rd. Or a river side trail from the old Aqueduct to Fisher Meadows. Or a pedestrian bridge over the Farmington river at the aqueduct site
32	Rt 44 East or West is problematic for Bike/Ped. State DOT doesn't plan for all modes of transportation, just trucks and cars.
33	Rt 177
34	Route 44, Route 167, and the bike path
35	Route 167 from Route 4 to Route 202
36	Route 10 is completely dangerous to walk or bike.
37	On Deer Cliff/Henderson Rd intersection and along of entire Deer Cliff rd -due lack of crosswalk, many people just walk/ride a bike on a narrow road with speedy vehicles.
38	On Country Club Road, walking to the library or to West Avon Congregation Church from Burnham Rd. The cars go very fast, and some do not stop at the stop sign at the intersection. I feel the speed limit should be lower, and there should be a sidewalk to help pedestrians. Especially when there is snow on the ground. There are always people walking on that road and the sidewalk ends at the library driveway.
39	On Burnham between Scoville and Country Club. Drivers drive faster than the speed limit and there are plenty of houses and neighborhoods where people would like to walk to the bike path, to the library or any of the schools near by. Or just to Luke's and some of the other local shops. Walking on Burnham is scary. A sidewalk or pathway (like there is on Scoville) would be great to connect everyone and make it more walkable. The high school track team also runs on that road and it would be nice if it was a bit safer for them. It would also be great to have a sidewalk or path on Country Club going from West Avon to the bike path for similar reasons above. It would increase pedestrians and cyclists going to the new town center via the bike path.
40	Old farms to town center last to Italian club. Anywhere in town to fisher meadows.
41	Old Farms Road. narrow shoulders.
42	No sidewalk, speed
43	No side walk , traffic speed, lack of crosswalk , inadequate lighting.
44	New Road and Huckleberry Hill Rd. For residents in this area of Avon there isn't really a safe way to get to get anywhere on foot or bike. The Farmington River trail, public transportation and shopping are all so close but not easily accessible on these roads due to road conditions and vehicles speeding. On both roads, there are a lot of blind corners and hills with no dedicated space for pedestrians or cyclists. Many cars speed through when they can't see what's over the next hill or around the curve. For a cyclist or pedestrian who is willing to risk their safety in the current conditions, it certainly isn't a relaxing stroll.
45	My children and I cannot safely walk or bike to any playgrounds within the Town of Avon due to lack of sidewalks and protected bike paths from our neighborhood to such locations.
46	Most every road in the community as folks drive quickly, our roads are curvy, and there are no bike lanes or sidewalks.
47	Many of us walk and bike New Rd, and Huckleberry Hill. There are no sidewalks. Speeding in this area is an issue as well as driving under the influence. I have lived here 3 years and there have been 3 bad accidents on curves caused by intoxicated drivers. I frequently see the little nip bottles used for alcohol thrown on the sides of New Rd. which indicates drivers drinking while driving. The speed limit is 25-30, many drivers do 50-60 and pass in the no passing lanes. These roads are narrow and winding. We need for the police to start pulling individuals over and fine them.
48	Main Street. No real side walks. Also lovely street beginning at the intersection at Albany turnpike - no sidewalks, and traffic is too fast/dangerous
49	Lower Old Farms. Crossing Rt10 at Old Farms (the crossing light was removed during the new bridge project. Stupid move by DOT).
50	Lovely Street, West Avon Road, Old Fatms Road. These roads (and others) do not offer an adequate shoulder. Additionally drivers are sometimes careless or inconsiderate of cyclists.
51	Lovely street is a main thoroughfare and the most dangerous and terrifying street in Avon to ride
52	Lovely Street has no sidewalks or a safe bike lane and a high rate of vehicle speed. My 4-year-old will be starting school soon at Roaring Brook Elementary and there are currently no safe options for us to bike or walk to school even though we are close enough that doing so would be preferable over driving. Safe alternate travel on these roads would also gain access to the bike trail and all the shopping available along those routes.
53	Lovely street and US 202 due to traffic speed
54	Lovely Street
55	Library- sidewalks only exist of west Avon road and it I poorly maintained, no other sidewalks exist to safely walk/bike with kids to the library.
56	Lack of sidewalks, fast speed of cars,
57	Lack of ready access from Nod Road area to existing bike trail.
58	Kids can't get to town playgrounds via a safe sidewalk, dedicated walking trail, or separated bike lane either on their own or with their grown ups. It just isn't safe.
59	It would be wonderful to be able to walk to the library from the Stony Corners/Cotswold neighborhood, but the lack of sidewalks on both Burnham Rd and Country Club Rd make it very unsafe to do so.
60	It would be great to be able to get to the town pool or public parks by walking on safe sidewalks or biking in separated bike lanes, but this simply isn't an option and most roads lack even a safe shoulder to use.
61	It is not safe to walk or bike with my children to Roaring Brook School because there are neither sidewalks nor protected bike lanes on any of the major roads leading to the school
62	I would love to ride my bike along huckleberry hill and new road. Huckleberry hill provides access to the Farmington river trail. Currently I have to drive to the trail. It's not safe due to speeding, lack of visibility and lack of sidewalks/trail. The same for new road which connects to huckleberry hill rd.

63	I would love to bike or walk to the library, but there are no sidewalks and no separated bike lanes and barely any shoulder on the major roadways where drivers routinely ignore the speed limits.
64	I would love if there were sidewalks along E Main St/US Route 44/US Route 202. This is one of Avon's main streets. It would be lovely to be able to walk or bike to all the shops and restaurants here. :) I do not currently feel safe walking or biking on this street. Drivers regularly drive over the speed limit along this street, run red lights, and drift outside of their lanes. I worry I would be hit if I walked or biked on the this street. There is also a bus stop along this route. Commuters should be able to walk safely to the stop. A sidewalk would also be helpful to anyone in a mobility device that needs to be use the bus. Thank you for having a survey. I appreciate the opportunity to make suggestion to improve my community! :)
65	I would like to walk and bike along Rt 44 in order to get the the shops and businesses along the way. There are only limited non-contiguous sidewalks
66	I would like to propose you look into adding a paved, or dirt trail for bikes starting at the corner of Blueberry Lane (Parcel ID: 1360057) and Brookridge Drive (Parcel ID: 1510071) through the Avon town owned space (Parcel ID: 3410030). Town of Avon GIS Map: (https://hosting.tighebond.com/AvonCT_public/index.html?basemap=Base%20Map&level=8&x=-8113569.938074522&y=5130283.236031695&idx=0%2C2%2C6%2C7%2C13%2C16%2C17&pid=3410030) This path would open up access for children to walk or bike to school without the need to go on dangerous roads (Country Club/Lovely).
67	I would like to bike from the bike path on Country Club Road to the Library or the Middle School/High School
68	I would like Main Street to be walkable for at least a mile each way, similar to Simsbury's Hopmeadow St. (from 202/Drake Hill Rd intersection to 202/Iron Horse Rd. The crosswalks are already there, it's just a matter of improving the design and some creative traffic management. Im sure the urban planning experts can come up with something.
69	I wish walking or cycling to elementary schools or the library was an option, but there are no sidewalks, no bike lanes, and barely any shoulder on busy roads where people routinely ignore the speed limit so I dare not do this with my children or on my own.
70	I wish there were an east-west road which was safe for biking! Arch rd and Country Club rd have no space for bikes, and the cars drive too fast to be safe
71	I wish I could bike more safely on Country Club Rd between 177 and 167. It's very dangerous, and kids are often walking or biking on it. There is almost no shoulder, and cars drive fast.
72	I run on - or ride my bike on - the bike trails virtually every day of the year. I access the trail heads at the Rt. 44/Lovely Street intersection for the trail heading into Collinsville, as well as the trailheads off of Arch, Country Club, Scoville, and Thompson roads. I live in the geographic center of Avon and so getting to the trailheads requires that I use roads that are dangerous to travel on by bike and foot. I do not have a way to get my bike to the trail heads by car, and even if I did, I would still run, or bike to them. I'm very interested in seeing bike lanes added to the roads that would connect safe travel from one bike trail to the other. Roads to add bike lanes include: Lovely, West Avon, Stagecoach (North-South). As well as Juniper, Woodmont, Country Club, Hollister, Scoville, Thompson, Arch (East West). These roads make the most sense for bike lanes because they would provide a grid of safety from all directions - north, south, east, and west.
73	I live on Oak Bluff on the east side of Waterville Road/Route 10 and find it pretty hazardous riding along Route 10 which is my connection to the rest of Avon. It would be great to have a bike lane but that's a major long-term project. In the interim, it would be great to have some signage to make drivers aware of the likelihood of encountering bikes and proper protocol. Most drivers are pretty good but I've encountered drivers who think bikes should only be on the shoulder (wide in some places but generally narrow to nonexistent) and drive accordingly. Some signage reminding people to stay at least 3' away from cyclists (is that a law in CT?), to be considerate, don't crowd bikes, etc. Likewise, cyclists should be reminded to be thoughtful and considerate, e.g., ride with the flow of traffic, don't ride side-by-side, just single file, use lights,etc. I'm envisioning a 3'x3' or 4'x4' sign with two flashing lights reminding both bikes and cars to be considerate of one another
74	I live in Peachtree Village - 1) sidewalks to access route 44 shops 2) complete the sidewalk between Darling Dr and the bus stop at O'Neils for transit access and access to shops east of O'Neils. 3) crosswalk lights where rails to trails crosses Arch Road - maybe 1 of 10 cars stop. 4) better routing of the rails to trails through the town hall complex PS. Great work integrating the new Whole Foods complex with the rails to trails - often walk to get groceries.
75	I find the crosswalk by the post office to the Avon Congregational Church is dangerous. I would like to see improvements made there.
76	I feel unsafe walking on Huckleberry and New Roads. There is often little to no shoulder, blind turns, and drivers do not respect the speed limit and often engage in completely reckless driving behavior. I feel unsafe walking on Rt 44 where there are no sidewalks. I feel unsafe biking on West Avon Road.
77	I feel unsafe on Country Club Rd going to Collinsville. The roads has many curves and drivers traveling too fast.
78	I cannot reach the Avon library from my neighborhood by walking or biking in a safe manner due to lack of sidewalks and lack of protected bicycle lanes.
79	Huckleberry Hill travelling south of the Firehouse to Orchard Rd I feel unsafe due to vehicle speeding and limited visibility. There is no dedicated part of the road for walking or cycling.
80	Huckleberry Hill road, Lovely Street, Country Club Road, Hollister, do not have side walks and are very very unsafe for pedestrian or bikers. We need side-walks and bike paths to the library, to all public schools, and the shopping mall.
81	From Arch Road to Sperry Park. You are pretty much mixed in with high speed auto traffic and it is a little scary.
82	Country club Road is unsafe to walk, bike or run. Our family would like to have sidewalks so we can walk to schools. We would also be able to access the library and the bike trail if that road was safer for pedestrians and cyclists.
83	Country Club road between lovely street and west Avon road. Lack of shoulder and fast moving traffic make it challenging to bike to Roaring Brook or Avon Middle School.
84	Country Club Rd. My kids could walk/bike to school, the library, sunshine market and luke's if there were sidewalks and crosswalks. Why are there several elaborately marked crosswalks for the country club, but none between the library and the church/daycare?

85	Country Club Rd. There is very little shoulder and cars travel at a high rate of speed. For the side East of 167, accessing the library, businesses, church, and rail trail is dangerous when coming from any of the connected neighborhoods (Tamara Circle, Winding Ln, Stony Corners, etc.). Intersection of Burnham Rd/Scoville Rd. Difficult site lines. A dangerous crossing for kids that would bike to PGS/TBS. Old Farms Rd. But this is getting fixed and the plan looks great with the inclusion of a gravel path along side the new roadway (both projects).
86	Country Club Rd, Arch Rd, Burnham Rd, W Avon Rd. None of them have bike lanes or even wide curbs and people speed on them. If those 4 roads were bike friendly, I feel the vast majority of the town would be accessible by bike.
87	Connect climax rd sidewalk further into neighborhood. Can't get safely from Whole Foods to fisher meadow.
88	Cars fly down Country Club Rd. and it feels dangerous to even attempt to walk or bike since there are no sidewalks or bike lanes and narrow road shoulders.
89	cannot walk from Farmington Canal Heritage trail on country club to interior streets. Sidewalk ends on country club by library instead of continuing up to walking trail. At a minimum, should connect to Stony Corners from walking trail like it does on Scoville. That provides a nice neighborhood loop. Traffic speed on Burnham is too much and sightlines are tough with trees that need to be trimmed back.
90	Burnham Road: There are sections where it is hard to see oncoming traffic due to the slight hills of this road, which feels especially unsafe without sidewalks and cars are often driving above speed limit. Would love to walk on this road more to connect to Scoville road to access Farmington Canal Trail or to head up to the library.
91	Burnham Road, Old Farms Road, access to library without having to go significantly out of the way
92	Burnham Rd. Traffic speed
93	Burnham between Country Club and Scoville could use a sidewalk. Same with Country Club. It is very dangerous to walk on. It would be great to connect to the sidewalks on West Avon and Scoville to reach the bike trail to get to the New Town Center and also have access to walk safely to the Library, the shops at the corner of West Avon and Country Club, and the schools in those areas.
94	Burnham and country club Roads
95	Biking or walking on New road to connect with Huckleberry Hill is dangerous due to a few factors; Limited line of sight on the hilly areas, curves in the road, speeding cars and in some spots there is no where to move off the road if a vehicle is passing too close. Specifically there is a section that a pedestrian would have to choose risking a fall into the river or planting themselves flat again severely sloped terrain. People do walk in this area but as a driver, even going very slow, there are areas where I can't see pedestrians until I come up right behind them This is why I go slower than the speed limit but most people do not. Being able to walk or bike safely along this stretch of road would be amazing and allow access to the trails along the Farmington and public transportation.
96	Avon old farms rd to Fisher meadows
97	Arterial and collector roadways in town are narrow and lack suitable sidewalks. That coupled with high operating speeds, volumes, and aggressive/distracted driving make the towns roadway network unsuitable to non vehicular travel. One of the few sidewalk connections in town on west Avon road has historically been in deplorable condition making it unusable. Avon's portion of the Farmington Canal Heritage Trail is also not maintained to the same level as adjoining communities, often finding it overgrown and uninviting.
98	Arch Road to Rte 44 and Rte 10 intersection. Love to trek to Bruegers off the bike trail. Last 1/2 mile not safe for several reasons (narrow, traffic speed, impatient and rude drivers)
99	Arch road is unsafe. Cars speed while texting. There is a small foot shoulder line which is not enough and it s full of dangerous pot holes, it also has sewer grates which a bike cannot travel over., For ing the bike to travel out in the road area. Very very dangerous to walk or bike in that road.
100	Anywhere. Roads are narrow and have no sidewalks, and traffic is insane.
101	Any business along 44 between climax and bushy hill. No sidewalks or other way to access other than risking life on rd
102	Along Rt 44 near the new Whole Foods - insufficient sidewalk in some places. You can take the bike path instead, but it's a longer way around to get some places.
103	Along HWY 44 through Avon. And along Country Club Road from Avon Old Farms to the Avon Free Public Library. There is no sidewalk for walkers.
104	Allong west avon rd. Connecting path to library fisher meadows all parks and new river access.
105	Access to the Avon Library from the rails to trails.
106	Access to shops on 44.
107	177/Lovely Street, West Avon Road -- speed, no sidewalks.
108	177. Holister. Country club.
109	lack of sidewalk and inadequate lighting,

Monthly Mobility Question #2 If you have school-age children, how do they get to school?

1	Would love to see more kids walking and biking. It's definitely easier for the kids near PGS / TBS. I have to believe there are a ton of families that would like to walk to RBS but aren't comfortable due to lack of sidewalks on Country Club.
2	With no safe walking or bike lanes, it's impossible to let my kids walk or bike to school
3	Wish they had the option to walk or bike to school in a safe protected manner.
4	What is the limiting distance children should walk to school ? Half mile ? What is safe ?
5	We would walk there more often if Burnham had a good and safe walking path. As of now, I am too scared for them to walk on that street
6	We would walk or bike with our kids to school if we could but with no sidewalks in country club road it is not safe.
7	We would walk or bike to school some of the time if we had a safe path
8	We would love to be able to walk to school and allow our children to walk on their own at older ages but do not feel safe doing so without the presence of a crossing guard on Scoville Road.
9	We would love more bike accessibility, larger roads with bike lanes, and cross walks on busy roads especially to roaring brook school
10	We walk to school almost every morning on the Scoville Rd sidewalk! And the kids take the bus home in the afternoon.
11	We need more sidewalks!
12	We live very near to roaring brook school and would have loved for our kids to safely walk or bike. We walked on occasion and were almost hit by cars. Next year, I will have a middle school child. We are biking distance to school but there is no safe way to get there so I will drive.
13	We are close enough to be able to ride bikes to school but would not feel comfortable riding on Country Club Road.
14	The walk to school is very dangerous in places, would love to see more sidewalks and main roads/avcess to town buildings and schools, library etc with sidewalk access
15	The roads kids could use to get to RBS on a bike are both too busy and too steep. Think about adding a bike path to the back end of Blazers trail to the intersection of Brookridge Drive and Blueberry Lane.
16	Thank you for considering how our children move around town. Dedicated bike paths (separate from the road) would be ideal to ensure their safety and ability to walk/bike to school, to friends houses and to local parks/library.
17	Please establish clear paths that distinguish bike/ ped paths from travel lanes. I understand the appeal of biking, but bicyclists do not always respect rules of the road.
18	Please do not spend countless tax dollars trying to get more kids to walk/ride bikes to school. It is impractical with our weather and climate, and it only favors those people who live in close proximity to the schools. By all means police speeders, but let's not build endless sidewalks, bike lanes, etc.
19	Please add sidewalks on Country Club Road!
20	No safe route to bike. Sidewalks (there aren't many) are really not for bikes.
21	My child has the ability to walk to school. We do not do so often as no crossing guards are present, there are a number of cars speeding, as well as many who do not stop when people are at the crosswalks.
22	More sidewalks for them to get to school safely
23	It would be safer to walk on sidewalk.
24	It would be interesting to survey the lifestyles of the parents whose children who currently ride to school, or a friends house. The current generation is not trending towards more outdoor activities. Outside of the obvious challenges that parents face with the digital culture, there is an over structuring of the remaining free time taking away from meaningful bicycling participation. Build it and they will come...but the question is, who is "they" ?
25	If there were sidewalks connecting neighborhoods like ours or separated bicycle lanes, my kids and I could walk or bike to school but this is simply not a safe option.
26	If there was a sidewalk on the west side of West Avon road from the middle school to the high school, I would not need to drive them.
27	I would love for my children to be able to walk or ride their bikes to school. It is not safe from our neighborhood. I don't like my children riding on busy roads.
28	I wish my kids could walk or bike to school but the roads between here and the school do not have sidewalks and some are not safe (too much traffic), so I wouldn't let them even if I were walking/biking alongside them. Avon REALLY needs to put sidewalks in on the busier roads (177, 167). People walk/jog on those roads all the time and there is so much traffic that goes very fast that it's scary to even see it.
29	I do not wish for my kids to take the bus, and unfortunately, driving is the only other viable option. I would love all the schools to be connected by bike trails, as well as an expansion of bike trails and lanes so that many more students could bike to school.
30	Even if sidewalks are added near schools, my children will not walk... Sidewalks would need to be added to the *whole town* to make that feasible. And, living 3-4 miles from the business center, we would never walk or bike to 44 anyway because that's too far.
31	Biking to school at this time would be impossible bc we are on Avon mtn (Wright drive).

Monthly Mobility Question #3 Do you support the addition of sidewalks in your neighborhood?

1	Would love to see a sidewalk from the library all the way down country club road to to the bike path (close to old farms rd)
2	We need sidewalks for safer pedestrian and bicycle usage. The roads I currently walk and bike on have less than a foot of shoulder on which to walk/ride, and it is NOT enough space. Cars will give a 3' distance sometimes, but not always - usually if they will have to cross the yellow line to give me the required space. So they blow past me at 60 mph in a 30mph zone with a foot of clearance. I'd shovel every sidewalk if it meant not having to deal with this!
3	We need sidewalk on Lovely St. I walk my kids to Roaring Brook School and it feels unsafe. Sometimes we have to walk on grass to avoid incoming cars.
4	We need more sidewalks to make it safer for kids to walk and bike! Also, is there an option to build sidewalks through town open space (like the Juniper open space) which would help connect neighborhoods to schools and libraries?
5	We live in a neighborhood so close to the commercial area and we still need a car to walk to those stores because it isn't safe to walk. We would love the side walk to connect us from Lawrence Ave neighborhood to Whole Foods area and Avon marketplace. On top of that, it would make possible for us to get to the bike trail.
6	We are on Thompson Road and between the 5-6th grade school, the walking path and Sycamore school there should be sidewalks on Thompson. Also on West Avon between Thompson Rd and Sycamore Rd.
7	This is a test answer from Danny.
8	The area around Roaring Brook School and along Rt 44 should be made priorities.
9	Thank you
10	Sidewalks would make travel safe.
11	Sidewalks would enable greater pedestrian mobility, and promote an enhanced community within and among neighborhoods. Vehicles travel too fast and or too close to pedestrians, which deter walking and cycling and further use of vehicles.
12	Sidewalks needs to be added for safety but also improved quality of life. It also improves business traffic
13	Sidewalks along key corridors are critical in providing safe walking/running routes for Avon schools and the general public.
14	Please, please, please add sidewalks on Old Farms Road from Sandscreen to Arch. The road is a speedway and is not safe to walk on.
15	Please install safe cycle or walking path along Hollister drive!!!
16	Please bring sidewalks up to Lawrence Ave!
17	Our taxes are high enough. Please do not fund this project and make them higher. There is also increased liability with sidewalks due to slip and falls.
18	Our street is a quiet side street and people walk on the road. It is not an issue. But if I lived on a busy road like country club or hollister, I would support having a sidewalk in my right of way. I would want to be connected to other side walks to get around town.
19	Only if they're in addition to bike lanes, not instead of them. Sidewalks are great for walking but not great for biking.
20	Love the plan. It will make avon an even better place to live
21	Live on quiet, no outlet street so don't need sidewalks. Would appreciate them on busier roads.
22	Lawrence/Francis Neighborhood Avon
23	Lawrence Avenue, Avon
24	Increasing accessibility (e.g. more sidewalks, trailhead parking, and trails), and improving the safety for walkers or bikers throughout our town will not only improve the value of Avon properties by encouraging a more healthy lifestyle, but it is also environmentally sound. As an Avon homeowner, I strongly encourage these types of improvements as an excellent use of tax proceeds, and encourage pursuit of any state or federal grants that may also be available to help. For elderly or other town residents that may be required to manage the sidewalks on their property without the ability or means to manage sidewalk maintenance, I would also encourage town subsidies to help. A great use of tax dollars! John Kallassy
25	I'd love to see sidewalls down 44. A way to get from the Lawrence ave neighborhood to the new shops would be great. Also going towards all the shops the other direction would look great as well.
26	I would love sidewalks, but I have about 40 yards of frontage on our street. That is a huge amount of snow clearing on top of clearing our driveway. That's 1.5 hours each morning for an able person with a blower. Plus it opens us up to liability if someone slips and falls. If we do not have to maintain it nor assume liability because it's town property, then great - please put in a sidewalk. Otherwise, absolutely not. In Europe, there are walking and biking paths along the roads. They are maintained by the municipality. Transferring liability and maintenance of a public way over to a resident is ridiculous in an area like this.
27	I think many community members walk and it would be so much safer to have sidewalks. I would love for my kids to walk to school but don't allow them to walk along Country Club towards West Avon too dangerous.
28	I suppose select installation of sidewalks could be part of the solution, but as an adult road cyclist, I find riding on sidewalks to not be the best. Raising driver awareness, signs, designated on-the-road bike lanes and incorporating cycle friendly design to the road from a planning perspective (road widenings, painted bike lanes, etc.) would be helpful.

29	I live on Devonshire Court (near W Avon Rd and Country Club), which among all other Avon streets and neighborhoods, benefits from being a 13 home cul-de-sac leading out to West Avon Rd, where there are sidewalks. That said, there isn't a cross-walk from where our street meets West Avon Rd. At times we have to wait minutes - and cross with uncertainty about cars that may be speeding through the nearby streetlight at Country Club Rd. Once we do get to the sidewalk they also greatly vary in quality as they end suddenly and re-appear on the other side of the street. Between degrading asphalt, overgrown bushes, and absence of crosswalks - although we have access to sidewalks, the sidewalks we do have are questionable at best. It certainly feels more like an adventure heading out for a walk as opposed to a casual stroll. We would love to be able to take a sidewalk all the way down Country Club Rd. to Fisher Meadows Trailhead. J. Spallone, 27 Devonshire Court
30	Having safe access is paramount to the long term vitality of the town. So we need sidewalks in key places to enable mobility. Given the size of properties/street frontage in Avon, I do think that we should consider changes to town regs and a means for supporting town services to clear such sidewalks. I think it is unreasonable for individual homeowners to be responsible for sidewalk clearing - we just don't have the density to make this realistic... it just won't get done timely. Or sufficiently to ensure safe access. Who's liability is it if someone gets injured on one of these sidewalks, anyway? Again, very supportive, but people need to know what it entails and what options are available. Let's have public discussion and workshops on these important topics, rather than try to summarize public opinion based on a few simple questions. It's just not adequate.
31	Fix the sidewalks you have first
32	Does not make sense in my residential neighborhood where mobility/safety is not a real concern (though there are still plenty of neighbors who drive too fast and blow stop signs with impunity)... but does make sense on primary and secondary roads that provide connectivity. Sidewalks are really about pedestrians, rather than bikes. It would be unsafe for shared use of a "sidewalk", no? As for snow removal... nope. Town should handle snow removal in the right of way. Trying to envision a plow pushing a 2-3-4 foot tall pile compacted snow onto a continuous sidewalk and expecting residents to get out and remove it. Investment in snow blowers...yikes... and the environmental impact of all those small engines... not to mention possible injuries to the residents... or liability if not done properly/timely. We should have a workshop to discuss all of this!!
33	But I think there should be one on Climax between the town line and the sidewalk that comes up part way from the Whole Foods development.
34	Avon DESPERATELY needs sidewalks. It is, honestly, absolutely ridiculous how few sidewalks Avon has in the residential areas, especially when there are so many very busy roads in residential areas (the 167 and 177 especially). We live within "walking distance" of Roaring Brook school but will never be able to walk our kids to school because there are zero sidewalks between our home and the school. We cannot walk ANYWHERE from our house. I would be willing to walk up to 45 minutes to walk my kids to the park, playground, or library, but it's impossible to safely get to any of those from where I live. This fact alone is making me consider moving from Avon.
35	As I mentioned at the town hearing, I live on Hopmeadow St. north of rt 44. I have a bicycle and use the incredible bike path frequently. However, as I bike into town from my house, the sidewalks end around the Congregational Church and Dom's coffee, and I am isolated from safely accessing a lot of the retail on rt. 44 that I rely on for shopping, eating and entertainment. More of the retail on rt. 44 must be connected to rt. 10, and interconnected to each other, via a network of safe and accessible pathways. This is a critical issue for equity and safety among many residents who do not possess a car.
36	about time

Monthly Mobility Question #4 Is there anything else you'd like to add regarding CT Transit, Road Safety, Bicycle accommodation & Parking, and Pedestrian Safety in the Town of Avon?

1	What north/south routes does CT Transit operate through Avon? How do you get to and from Rt 44 and Rt 4 by public transit?
2	We've lived on Thompson Rd (on the West Avon Rd end) for almost 30 years. We DRIVE to Thompson Brook School or the Walking Trail Parking lot as it's too dangerous to walk on our road given the speeds people drive on it. It's even more so now with the significant increase in traffic due to the school drop offs & pick ups. We joke that we can't have any emergencies from 8:00-8:15 am and 3:00-3:15pm as there's no way First Responders could get to any of our houses due to the parent traffic. Also, the traffic signals (mailing the red lights) at the intersections at Rt44 & Dale Rd, and, Rt. 44 and West Avon Rd, are routinely ignored by traffic with no enforcement in site.
3	Weekday bus service to Hartford is actually pretty good, but there's no service on weekends so that's why I'm dissatisfied. I haven't noticed many bike racks around town, but I can always find an unofficial bike parking spot. Biking on the major roads isn't great, I want bike lanes.
4	We'd love to see country club road and old farms road area more pedestrian friendly
5	We need safe ways to access businesses, schools and parks by foot and bicycle.
6	We need more sidewalk in areas of public service: library, churches, stores,
7	We desperately need bike lanes on major roads to support walking and biking to shopping and retail establishments in town.
8	Walking in Avon is treacherous bc drivers speed on local roads. Country Club road bisects part of the town and there are only a handful of areas to safely cross the street on the entire road - which is a few miles long. I can't walk my dog across the street because it is too dangerous. I have been told in the past by town officials that there is no way to control traffic flow on the street (no stop sign or speed bumps allowed) which makes zero sense. I grew up in a city with a stop sign at every street corner. Kids walked places! Even painted crosswalks along country club at the cross streets would help. Cars simply have no expectation to stop to allow walkers or bikers to cross the street.
9	Vehicles frequently go over the speed limit on Burnham Rd, taking it as a short cut north. Sometimes they do not stop at the stop sign at the intersection with Country Club Rd. From my yard I have heard many screeching brakes. With no breakdown lanes and poison ivy along the sides of the road, it is a dangerous place to walk. For residents it is necessary to quickly get to a safer road.
10	Two specific things on the top of my mind recently. We walk our daughter to Kindergarten every day at Roaring Brook School from our house on Old Wheeler Court. There is a sidewalk at the end of our road that ends almost immediately. The sidewalk resumes on the school property. It would be really helpful to connect these two sidewalks and it's only a very short distance of additional sidewalk needed (roughly 50 feet?). The second suggestion is to install a bike rack at the front of Roaring Brook School, where drop off happens, so we could bike to school, and lock up the bike there during the school day. Thanks for listening and for working on this great initiative!

11	This is not a walking town. The speeders make safety a real issue. There are very few sidewalks. While there are very few safe places to walk more should Be done to control speeding More signs that there are pedestrians and street stop signs at intersections to allow pedestrians to cross where there is no traffic light.
12	There's a huge opportunity to add wider shoulders (by narrowing travel lanes), add road markings, etc. to improve safety for bikes and pedestrians. As for buses, I can't imagine it could be much worse. From what I know the only bus route runs along Route 44. That's not at all accessible for most people.
13	There must be a sidewalk on the west side of 167 between the middle school and high school. It is not enough to only have a sidewalk on the east side. Kids often cross the road to go to the gas station or Da Vincis. It is unsafe without having a sidewalk on the west side of the road.
14	There isn't any signage around Avon for bike - Ped. Awareness ... i.e. "beware of bicyclist"... 3' Law, Share the Road, Sharrows,... Etc.,
15	The town simply does not have pedestrian or bicycle friendly infrastructure connecting the majority of residential neighborhoods to town facilities, schools, or major commercial areas. It also lacks any reasonable way, other than driving, to make public transit accessible unless you happen to live along Route 44. This has to change.
16	The roads are relatively safe for cars (save Old Farms Rd) but I don't feel that they are safe at all for biking or walking. I feel that I should be able to ride or walk with my children to one of the many parks, recreation areas or bike/hiking paths in the town, but it is not safe at all to do that. The cars drive well able the speed limits even when driving through neighborhoods with houses, as well as on main thorough fairs. Even if they did maintain the speed limit on roads like Rte 10 or Rte 44, Old Farms or West Avon Road it is not safe for a biker or pedestrian. There needs to be a parallel trail or sidewalk for bikers and pedestrians.
17	The path along West Avon Road is overgrown with tree branches in spots. The path pavement is uneven and unsafe to jog or even walk on. I popped a tire 6/27/23 as the path ramp ended in a pot hole (south of the gas station.) I was trying to bicycle to the library- which I can't wait to safely do! I am so excited that we are taking steps to make Avon a bicycle/walking/wheeling friendly community!
18	The lack of a properly paved sidewalk connecting all the schools along the west side of west Avon road down to sycamore hills recreation area is a glaring gap in bringing communities together and increasing the ability of a large number of kids to walk to school.
19	The infrastructure that exists is not maintained. Before expanding take care of what is there, utilize existing resources to enforce speed on roads to ensure safety, repair and increase pedestrian crosswalk signage on roads, repair sidewalks. Expansion is a nice to have, but doesn't matter when what is currently there isn't maintained/ enforced.
20	Sidewalks sidewalks sidewalks!
21	Sidewalks on country club road; at a minimum to connect neighborhoods to the rail trail.
22	Safety traverse RT 44, so that shopping can be done easily.
23	Road safety in Avon could be improved. There are so many unsafe drivers that speed on twisty roads where there is no space for sidewalks and limited visibility. These areas should have some type of speed bump installed with a notification that there are pedestrians present.
24	Please create more sidewalks, at least, for walkers along main routes.
25	Pedestrians and bike riders need to be held accountable for their failure to follow the laws. Their poor decisions, ignoring lights and signs, walking / riding in front of cars, is out of control. It's because whenever anything happens, drivers are blamed when it's usually the walker / rider who is at fault. They don't care about the laws or rules and they are supported by law enforcement.
26	Pedestrian Safety: I feel the intersection of Country Club/W.Avon needs improvement bc all the school kids are using the walk signs and crossing there. And they have to wait too long for the crosswalk to signal. They are in groups and playing around, I worry drivers will go right on red and injure them. When the high school & middle school dismiss there are lots of kids walking between those schools and drivers need to slow down and it would be great to make the sidewalks safer somehow.
27	Passiveness/missiveness of APD not enforcing laws that would improve safety. Ignorance and inconsiderate behavior with regard to right of way that puts pedal/bikes in danger. It's not the responsibility of oncoming traffic to get out of the way so that oncoming traffic can pass a bike/ped. It's your responsibility to yield to the bike/ped and wait until it is safe to pass them.
28	Our town falls very short with the lack of sidewalks, and if we do have them they are in horrible condition. It's embarrassing. More sidewalks by schools. Probably a few speed bumps near AMS so kids can safely cross. Maybe even a median at the intersection. It gets so busy there and we should encourage our kids to walk/bike but it's crazy after school. It would also help support the businesses there.
29	our needs involve more than bicycle issues... the focus needs to be very much on helping those people among us who have no choice but to take the bus to and from work....we must get shelters/benches at the bus stops...
30	not many motorists respect the rules of the crosswalks, and some even blow past the stopped cars and speed through the crosswalks- luckily missing the pedestrians/cyclists. Speeding and providing the 3 ft distance from cyclists are not enforced. But also some pedestrians and cyclists make it dangerous for vehicles when they ride against traffic or walk with the traffic- education/enforcement to both vehicles and pedestrians can be helpful. Need more time allowance for crossing pedestrians on the Country Club Rd intersection, it is on a blind turn. Flashing lights should also be installed further up from the intersection, hopefully motion sensor- cars speed through there regularly and doesn't allow reasonable time for the speeders, if speeding itself is not enforced.
31	Need better biking/walking options. Would like to see more options on Old Farm Rd - specifically in the areabetween Hop Meadow brewery/Fisher Meadows
32	More traffic enforcement is needed to stop this trend of lawlessness on the roads. We can't engineer our way out of this because there's too much to fix.
33	More sidewalks along Rt 44 and secondary Rd. Wider road shoulders(bike lines) near bike path and other roads. Rt 44 needs a walk/bike path between shopping plazas. It would be good to have a coloboration going with Simsbury and Canton regarding wider shoulder/ walking/biking path.

34	It would be great if a campaign were launched to educate residents that it is not safe to "squeeze bikes and pedestrians that they don't have the right of way to and it is not the responsibility of oncoming traffic to slow or get out of the way when they swerve into the oncoming lane. It is no different than when a mail or trash or delivery vehicle is in their lane. Behavior adjustment would cost far less than infrastructure. A Weekly or Monthly Safety reminder campaign... This could be a great thing if we had just a little bit of money to put behind it!! Dom's had to take it on themselves and people have to buy signs. Use our tax dollars for efforts like this that are for civic benefit! "Be the change you wish to see in the world" - Rumi Also - policy should enforce laws - for instance, it is not ok to put your trash bins in the shoulder of the roadway on pickup day. This is a constant problem on West Avon Road between Dale and Arch Roads, for example.
35	I'm glad traffic safety is starting to be seriously addressed, but we need SO much more in order for our town to be safer. People speed, run through stop signs and lights, drive way too close to pedestrians and cyclists, and just seem unaware of how dangerous their behavior is. I live off a main road, with a 30mph limit, and regularly encounter people going 45-50!
36	I would love to commute by bicycle to my job in Canton but that requires travel along Rt 167 and Rt 44. I do not feel safe enough to do this.
37	I would like to see a bike route that connects the Huckleberry Hill side of town, to the Avon bike trail where you do not have to go all the way through Farmington to access it.
38	I would like to add the need for sidewalks on Lovely Street. I live just off it on Nottingham Ridge I occasionally need to walk to Unionville. It's very dangerous until you get to the Farmington line where sidewalks begin. All the major streets in Avon strike me as very dangerous for bicycling. I do see people on them on occasion. I cannot believe there haven't been more fatalities.
39	I run on or bike on Avon roads nearly every day of the year and I've realized that distracted driving is the largest threat to pedestrian safety. I would love to see more enforcement of distracted driving laws on west avon road and lovely street. A large percentage of drivers are not paying attention (mostly looking at their phone). When I run on those roads, I usually count all the cars and count all the times the driver was distracted. The average tends to be around 15%-20% of the drivers coming at me are distracted. That's not acceptable.
40	I live on Millstone Dr and turning left onto Hollister is incredibly DANGEROUS as its a completely blind corner around a steep bend. All the neighbors and visitors agree. To turn left from Millstone onto Hollister "safely" the only option is to turn into the oncoming traffic lane then switch to the correct lane once you see no one is coming around the corner. There have been several accidents on this curve and currently a tree is missing bark from where a car must have swiped it. I've had many terrifyingly close calls myself. The line of sight needs to be improved, forcibly remove plants from the home on the corner blocking the view and widen and straighten the road. I had called town hall who said to contact the police who really have nothing to do with this issue so we don't know where to go next? The police said they don't add mirrors but that would also help so we could better see potential oncoming traffic. THIS CORNER IS A DEATHTRAP!!!
41	I live east of RT 10 on Cider Brook. I do not understand why DOT removed an existing crossing light from Rt 10 as part of the new bridge project. Technically I'm not allowed to cross RT10 to get to Fisher Meadows and the bike trails. A step backwards.
42	I feel that there is minimal pedestrian and bicycle opportunities for Avon residents. I recently visited Europe and it opened my eyes on how far behind we are in CT when it comes to the walking and cycling trails
43	I don't think Avon needs to add in bike lanes or bike paths.
44	I do not take the bus and I bike ride only on the trail so really have no opinion on most of the questions.
45	I can not walk from near my house to the Unionville Townline. I fear for bicyclist. On Lovely Street.
46	I attended public meeting June 20. We were asked to vote on 3 options but I would suggest that a combination of solutions should be explored. A single option will not work for every street in Avon. I live on a cul-de-sac with 20 homes. There is no need for a multiuse path on this street (Hitchcock Lane). However on West Main Street a multiuse path would make sense.
47	Crosswalks are disregarded by many drivers, especially at Pine Grove School. Turn the school commuting times can't there be a volunteer crossing guard?
48	Bike chevrons and lanes along major road routes
49	Avon needs sidewalks. The lack of sidewalks in Avon is ridiculous. I do not understand how there are literally no sidewalks except in like 2 small areas. Almost none of the residential areas have sidewalks. The only explanation I can think of is that Avon is full of rich people who think walking is for poor people and want to get in their giant SUVs and drive to their country clubs and not have to mingle with commoners. It is very difficult to build community and get to know your neighbors without sidewalks. I feel like Avon is lacking this type of community cohesion because it's difficult to just walk around most neighborhoods, especially if you have kids and don't want to walk in the road.
50	Avon is, simply put, a car dependent community. In my part of town, and most of town, there are no sidewalks or even reasonable shoulders on major thoroughfares, so it is not safe to walk or bike as an alternative to driving. And bus transit is only available in town along the Route 44 corridor, or, in my part of town, a 35-40 minute walk to Unionville. And even then, one has to go all the way to Hartford just to get back out to the Route 44 bus route. So it simply isn't a viable alternative to driving to get around Avon.
51	Avon does not need to be a transit center. This is what Gov Lamont and the state wants so they can force affordable housing near the transit center
52	Arch Road connects the town to the bike path, but there is no shoulder, let alone a sidewalk. I can see people parking at Church of Saint Ann, then riding to the bike path, maybe taking a left turn under Rt 44 to the Whole Foods shopping area, or continuing to Old Avon Village, or Sperry Park. Need a light at Arch Rd and Old Farms Rd.
53	Although I'd like to see more roads that are safe for pedestrians and cyclists, there are some roads where they should not be allowed - Old Farms Road is one, Route 44 is another. If a completely separate path/trail were constructed, then it would be safe but Old Farms Road is too narrow in most places, people drive like idiots on it and speed limits are not enforced in the dangerous portions. Route 44 has far too many business entrances and exits for it to ever be a safe place to walk and bike. Speed tables or rumble strips should be put in the winding areas of Old Farms Road. Large trucks should be prohibited from using that road - just as they are from the Merritt and Sawmill. Enforcing the speed limit on Old Farms by Fisher Meadow and the industrial area at the other end is not nearly as important as enforcing it on the winding scenic area. Many people accelerate for those turns, rather than slowing down. It would be a safe road if people went the speed limit.

54	Although I am dissatisfied with the above issues, I do think we have a good start, but can do much better. Residents still need to use a car to access most businesses safely. Think about linking shopping areas together with safe access from our main roads and neighborhoods. Also I realize that bus ridership is low. I sometimes walk to the bus stop in front of O'Neils to get to and from the train station because I don't want to leave my car there. There are many empty seats. Would Avon residents use the bus to get other local areas if there was more and more convenient service. Not sure. Thanks for the opportunity to comment.
55	A speed limit sign on Old Farms Road EASTBOUND between Tillotson Rd. and Waterville Rd. (Rt. 10). Although the winding road through the woods is posted (25mph), Eastbound drivers behave as if once they come out of the woods, and leave the AOF campus, and they can see ahead our beautiful new bridge over the Farmington, that the straight-away road along Fisher Meadows they accelerate quickly and pick up speed! This is extra dangerous because it is a destination Recreational facility, with driveways to parking areas, playing fields, canoe launch, children, bicyclists, and runners. There is one lone posted speed limit sign on the westbound side of this stretch, but even that seems to me to be located much too close to the intersection that drivers do not notice the sign when turning onto Old Farms from Rt. 10. Could an additional sign be place more where it would be more easily noticed, maybe near the entrance to Fisher Meadows? So, additional signage, in both directions? Thank you!

What other or more specific needs should the Town of Avon take into account when considering bicyclists, pedestrians, transit riders, and wheelchair users as part of our planning process?

1	With the right thought process for shopping, getting mail, running for dinner etc. having safe lanes makes all the difference, I use my bike on 44 going to the a few appointments, it's an adventure and I might add very unsafe. Using the bike path from Canton to Avon, has no direct route, and no marking on the other secondary roads that are high traffic.
2	Whenever possible, having a separate surface from the road, giving a nice, wide bike/breakdown lane on through-ways, having more crossing signals, and traffic cams at as many lights and crossings as possible.
3	We should have a median on 44 as it goes through old Avon village - say Max a Mia or Barr 44 up until Whole Foods. Farmington did it from Their historic part until Farmington Country Club on their busiest road why can't we? We need to cultivate and hold On to the one part of town that doesn't look like a tacky strip mall (other than small section by middle school Which we should build up a little more too)
4	We should be doing whatever we can to break our dependence on car travel. Bike infrastructure and public transit are desperately needed.
5	Walkers, cyclists and wheel chair users should be able to get safely from their home to at least one store for necessities. Children should be able to walk safely to school.
6	Walkers / riders have responsibilities to ensure their safety, it should not fall solely on drivers. These people need to be held responsible for their failures. Furthermore, they need to understand that "tight" routes should be off limits during certain times of day. Riding a bike through AOF at 4pm in the middle of the lane is just bad decision making by the riders.
7	Traffic control in this town is needed. See above. Kids walking on main roads to get to/from middle school and high school are hard to see while driving bc of the hills, twists, turns, lack of sidewalks, crosswalks, lack of stop signs. Cars are speeding by in excess of 50mph on my street. I can't believe we haven't had more pedestrian tragedies. I've all but given up trying to cross the street I live on. I drive short, walkable distances instead.
8	To accommodate people with disabilities, sidewalks need to be installed everywhere and some form of mass transit or bus for disabled (not just seniors) need to be implemented in town. As far as bike paths...not all roads are wide enough to add them and bikes and pedestrians should be banned from those roads. Many are windy and this limits vision. Pedestrians seem to think they can do whatever regardless of whether it's safe/unsafe, and it's always the driver's fault... However most wear dark clothes at night...walk next to each other and get too close to where cars drive, bicyclists don't have bike lights, people let their dogs out on leash too far ahead of them to control in an emergency (let's not talk about kids!), and walk up and down roads where pedestrians should not be. Speeding is not the problem in town. People/bikes being too close to passing cars is.
9	This town NEEDS sidewalks. The narrow windy streets were designed when the town had a much smaller population. There are far too many cars now for the road to be safely mixed use for pedestrians, dog walkers, bikes, etc. To keep everyone safe we must add sidewalks, sidewalks, sidewalks!!!
10	This effort seems to focus largely on recreational aspects of transportation, especially focus on bicycles and recreational trails. Please do not neglect the needs of some of us to move about for meeting daily needs and get to community facilities without personal vehicles some from preference some from necessity.
11	There needs to be a pedestrian sidewalk on Country Club, between West Avon Rd and Lovely St. The western grade from Carriage to Wheeler Rds should be reduced to support motorized wheelchair users.
12	There could definitely be more crosswalks and crosswalk signage.
13	The town has done a nice job putting benches along the rail trail and other areas. Please consider periodic benches or other sitting areas along any new route
14	The private property rights and liability of Avon's home owners. When you install sidewalks, the homeowner is now responsible for maintaining them and is liable for slip and falls that happen on them although we are not responsible for repairing them. So an uneven sidewalk panel installed and not properly set by the town or its contractor, that results in a fall to a pedestrian will result in a homeowner being sued alongside the town. Our legislative body should not be placing additional liability on our homeowners. Regarding bike plans, please consider that many cyclists don't understand that the street signs apply to them. They constantly go through stop signs, disregard lanes and turn signal requirements. If our municipal police department intends to equally enforce traffic laws upon them, than okay. However I doubt that cops are going to be stopping errant children and issuing them a summons when they run a stop sign and cause a car accident.
15	The lack of a sidewalk on the west side of 167 from the middle school to sycamore pool is a huge oversight and leaves a large portion of residential communities on the west side of 167 without safe sidewalk access making it impossible to consider waking to school or walking to the pool.
16	Sidewalks sidewalks sidewalks!
17	Sidewalks along major roadways such as Lively Street, Country Club, and West Avon Road through the entirety of town so we have some hope of having safe means of walking to major town facilities. We also need protected bicycle lanes on these same roads to make bicycles a reasonable and safe option. We have prioritized automobiles over people for too long.

18	SAFETY and to make absolutely certain these bike/walking lanes are maintained and clear.
19	Route 44 is a nightmare and I will not ride or walk it. It would be nice to see a bike trail that runs parallel with Route 44.
20	Repair what you have first
21	Prioritize bikers/walkers so that we can have less cars on the roads in this beautiful town
22	Possibilities to get to the trails by foot or using your bike. It is not ideal to have to take your car to have a bike trip (when you have to transport your bicycle to the trail)
23	pavement signage on roadway shoulders to remind vehicles to share the road. additional flashing lights at pedestrian crosswalks with better placement for cyclists and wheelchair users, the use of motion sensors to account for those who don't opt to use the flashing lights and allow more warning time for drivers. Wider paths or dedicated bike lane to account for both pedestrians and cyclists moving in both directions of the path. On busier days it is very difficult to pass walkers on a bike when walkers tend to walk side by side and when there are people traveling in both directions simultaneously
24	Other towns have posted signs to help educate the community about sharing the road (keep three feet from bicycles, etc.) We can invest in educating everyone about road safety (students riding to school, pedestrians/bicyclists, etc. as well as drivers.) Lighting. (Street lamps along W Avon Rd were intermittently lit last fall-winter-spring. Lots of dark spots. (need to see those bears lol!) Blinking crosswalks for children and students? Near the schools, parks, library.
25	Most shopping options are not accessible to bike. The trail is great and provides access to only one small part of town.
26	More sidewalks!!
27	More reflective plates behind traffic signals wherever possible? I believe they are a great addition, and genuinely help drivers to recognize that they are approaching an intersection. https://safety.fhwa.dot.gov/provencountermeasures/backplate.cfm Our trees along Avon streets are great at adding shade (etc.!) but also make it sometimes difficult to recognize upcoming traffic lights. Where they have been installed around town these past few years, they are wonderful! Thank you!
28	Make the Town LOOK, like a Bicycle Friendly Community!
29	Keep a close eye on how bumpy the trail gets from roots uplifting.
30	It seems there isn't a cohesive map to show all the safe routes for pedestrians/ cyclist to get from one area to another. For example once the trail ends at the town department of works, where does the the trail pick up again? Is there sidewalk access or will I be on the road for part of the journey? If so, are there pedestrian crosswalks to create safe passage? How can I safely walk/bike to stores in town? Clearly marked paths, or an accessible map would clarify these issues?
31	I would question the required width of a multiuse path. The heritage trail as it passes by Whole Foods seems narrower at points than the width suggested at the meeting. Also what is a side walk by definition? I generally think of it as a concrete sectional path about 3 feet wide. Multiuse paths I perceive as black top 4-5 feet wide that a continuous.
32	I see pedestrians trying to cross on Rt44 /Dale Rd and Rt44/near Friendly's and it's not as safe as it could be. Sometimes too there are pedestrians crossing Rt/44 at Walmart Plaza across to Goodwill plaza...
33	I see individuals walking along West Avon Road especially from Arch Road north to Rt 44 and it scares me that they will be hit by a car. Avon has few sidewalks so people drive everywhere. One area is around the schools - Pine Grove, Thompson & High School to improve sidewalks there as children use them. Maybe get those flashing walkway in-ground lights. They also walk to the Avon library from school. I would love to see a more natural style walking track like on the Thompson Road parcel for people to get exercise and their steps in for the day. I would like to go to the high school track but I don't go there as it is too congested and busy. Many hiking trails and the rails-trails have too much wildlife When the town was considering putting the turf field at Thompson Rd they considered outlining it with a walking track. Build recreational facilities for older adults who remain in town after their kids graduate fr HS. thank you
34	I live nearby Riverdale farms and am an avid biker who takes advantage of the town's great rail trails as much as possible. I also like to get to Nod Road and Bushy Hill Road, but the sidewalks that exist on Hopmeadow disappear at the base of the hill, and Rt. 44 is unsafe to bike on in either direction. There must be more safe sidewalks on rt 44 or a network of connected access points set back from the road between the retail plazas. Rt. 10 to Avon Old Farms should also have a bike lane, as well as Country Club Rd to access Avon Public Library and the high school.
35	I have noticed that one of the more dangerous areas for bicycle paths are often being ignored in many cities around CT. Junctions are scary, wide, and are stimulating making it difficult for motorists to notice cyclists. Often cycling lanes continue straight through a junction or are placed in between turning lanes and a straight lane such as in New Britain at the junction of Main St and East Main Street. Every time I'm in that area cars are in the cycling lane without looking for cyclists. When I traveled to Poland this summer, I noticed that when bicycle paths were not grade separated with the road, before a junction they merged with the sidewalk off the road which allowed some separation and better visibility for motorists and cyclists and improved sight lines such as in Warsaw Poland at the junction between Spacerowa and Belwederska. This felt very safe even though the lane was not separated the entire time. If you would like to reach out, my email is xpilka@yahoo.com, Thanks
36	I don't really know but I think that in most cases, an entirely separate trail/path is what is needed and safest - not just a wide shoulder.
37	Getting kids to bike and walk to/from schools should be a priority; cut down on pollution of all the idling vehicles for drop off and pickup. Or add monitors to the school buses so more kids use them.
38	Farmington trail is great, but trying to bike to other areas in town, like library and schools is difficult and dangerous. Most don't bike because of safe routes and access
39	Drivers have learned there are no consequences for speeding or going thru red lights in Avon (and Connecticut as a whole). Until there are consequences/fines imposed for breaking driving laws, bad behaviors will only get worse.
40	distracted driving signs, share the road signs, bike lane paint
41	Crosswalks are disregarded by many drivers, especially at Pine Grove School. Turn the school commuting times can't there be a volunteer crossing guard?
42	Busses should be on country club to allow for more ability to use bus routes. Currently very limited and you need a car to get to them.

43	Bike lane markings.
44	Biggest improvement would be sidewalks consistently along major thoroughfares to at least connect neighborhoods to schools and other town facilities. Protected bike lanes would also be a huge help on these major arteries. And for mass transit to be a viable option for Avon residents within town or the Farmington Valley, stops need to be walkable from neighborhoods and routes need to go within the community and between communities, not necessarily requiring one to walk 40 minutes to Unionville to take a bus to Hartford in order to take another bus to get to Route 44 in Avon. For example, it would take me 90+ minutes to walk to Walmart from my home, or I could take the bus with all the long walks involved and it would take me over 2 hours. Neither is a viable option for a shopping trip in lieu of a car.
45	Add stop signs and pedestrian crossings lines on the road where there is no traffic light

PUBLIC MEETING NOTES

First Public Meeting | April 25, 2023 | Avon Bicycle and Pedestrian Master Plan | 1st Public Meeting | Avon Senior Center | Meeting Notes

Attendees

Town of Avon

Brandon Robertson – Town Manager

Grace Tiezzi – Assistant Town Manager

Hiram Peck – Director of Planning and Community Development

Chief Melanson – Avon Police Department

Lt. Schmalberger – Avon Police Department

Ruth Checko – Director of Recreation and Parks

Avon Town Council Members

Dan Polhamus, Chairman

Heather Maguire

Anthony Weber

Barbara Ausiello

SLR

Holly Parker – Senior Transportation Planner/Project Manager

Dave Sullivan – US Manager, Transportation Planning

Town Council Meeting welcome and call to order – Dan Polhamus, Avon Town Council Chair

Presentation – Holly Parker - SLR

The presentation began with an explanation of the Avon Bicycle and Pedestrian Plan's purpose: to better connect Avon's existing bicycle and pedestrian facilities to each other and make them safer – not only for those who want exercise or recreation, but for those who use their feet or human-powered wheels for transportation.

AVON
CONNECTICUT

Public Meeting

Bicycle & Pedestrian Master Plan

Attend in person:
April 25, 2023
7:00 p.m.
Avon Senior Center
635 West Avon Road

Scan or click to learn more
from the Project StoryMap!

<https://arcg.is/0851im>

Town of Avon, Connecticut <https://www.avonct.gov/subscribe>

BICYCLE FRIENDLY COMMUNITY status is also being pursued by the Town. The two initiatives will dovetail into a body of recommendations that should serve to make this community MORE friendly for people who walk, bike, wheel, scoot, or skateboard.

An Advisory Team for the Bicycle and Pedestrian Master Plan was assembled this winter, led by Avon's Planning and Community Development Office, and including representatives from the Town Manager's office, Public Works, Recreation and Parks, and the Police Department – coordinated by SLR Consulting.

The presentation shared what the consultant team learned from maps, previous studies, field work, meeting with Bike Walk Avon, and compiling responses to "Monthly Mobility Questions" accessible through the project's StoryMap website and on map boards at the library and at the Senior Center. In addition to posing Monthly Mobility Questions, Avon residents were asked (by means of the project Storymap website and promptings by email and social media) if they had feedback on the project's vision statement, drafted as follows:

The Town of Avon, CT's transportation network will provide improved mobility, connectivity with existing trails, public health, and recreational opportunities. It will enhance sustainability by encouraging nonmotorized or public transportation options as an alternative to motor vehicle travel.

It was revised with feedback from the community:

To facilitate public health, recreational opportunities, and better air quality, the Town of Avon strives to improve access and safety for all who use our road and trail networks including bicyclists, pedestrians, and motor vehicle operators. To achieve this vision, the Town may invest in infrastructure such as sidewalks, bicycle facilities, traffic calming measures, and programming such as road safety education for all users.

The project's timeline was discussed, followed by a series of slides showing the existing conditions for bicyclists and pedestrians in Avon – including the Farmington Canal Trail, sidewalks, crosswalks, bike racks, recreation areas, and the locations of bicycle and pedestrian crashes. The existing conditions for transit users were also discussed, as transit is used in conjunction with walking and biking to enable longer trips than could be accomplished by foot or bicycle alone. It was noted that CTtransit buses are equipped with bike racks.

The presentation turned to that of public input received to date, and a map was shown that illustrated locations Avon residents feel unsafe walking or bicycling – which helped bring the areas of highest concern into focus. Included on the map were locations of proposed or planned improvements, showing that once completed, the proposed improvements to Country Club Road and Old Farms Road will address many of the community's concerns about those roads.

Some of the Avon community's written comments were shared – including a map sketched out by an Avon resident who suggested north/south and east/west routing for bike routes, as well as what destinations can be connected by those routes, and the floor was then opened to questions and comments from meeting attendees.

The next few slides showed recommendations from prior bicycle and pedestrian-related reports and studies, including a study of Route 44 from 2000 conducted by the Capitol Region Council of Governments and the CT Department of Transportation, and some of the recommendations that related to bicycle and pedestrian infrastructure in the Town's most recent Plan of Conservation and Development (2016).

The last slides provided a breakdown of the funds budgeted by the Town of Avon for transportation projects relative to funds available through grants, and the project's next steps. The point was made that most grants require at least a 20% local match – so budgeting funds with which to match grants is critical to making bicycle and pedestrian infrastructure projects possible.

Next steps include the gathering and analysis of additional public input through Monthly Mobility Questions on the project's StoryMap website and through email and social media, and the development of preliminary recommendations. A second public meeting will be scheduled in June and recommendations will be refined with community feedback and in collaboration with the Town Staff and Study Advisory Committee. As a last step, the plan will be presented to the Town Council for approval.

Comments:

- Sidewalks should be wide enough for bicyclists and pedestrians to share.
- Signage – such as "share the road" may be a low-cost and implementable first step to create bike routes.
- Conflicts between bicyclists and pedestrians on the Farmington Canal Trail make it challenging to use in the peak season.
- The project should share & recommend the best practices in creating safe routes to schools.
- Bike buses are a possible solution – with police escort.
- Funds are available from the US Department of Transportation through the "Safe Streets for All" grant program.
- E-bikes' speed must be addressed, as they travel up to twice the speed of a pedal bike and significantly faster than pedestrians – policies must address where and how they can safely integrate into the transportation and recreation network.
- Short-term/quickly implementable solutions should be proposed.

- Route 44 creates a barrier in the town's primary activity center.
- "Bicycle Superhighways" are being built in Europe.
- Burnham Road should be prioritized – as it connects schools.
- There is no safe parking area at the trailhead to Heublein Tower on Nod Road.
- Traffic calming can and should be done with measures such as chicanes, flex posts, and speed cameras – a "Vision Zero" bill is pending that may make some of these solutions more practiced/available.

Second Public Meeting | June 20, 2023 | Avon Bicycle and Pedestrian Master Plan | 2nd Public Meeting | Avon Senior Center | Meeting Notes

Attendees

Town of Avon

Brandon Robertson – Town Manager

Grace Tiezzi – Assistant Town Manager

Hiram Peck – Director of Planning and Community Development

Lt. John Schmalberger – Avon Police Department

Avon Town Council Members

Dan Polhamus, Chairman

Heather Maguire

Barbara Ausiello

SLR

Holly Parker – Senior Transportation Planner/Project Manager

Fiona Flynn – Transportation Engineer

Dave Sullivan – US Manager, Transportation Planning

AVON
CONNECTICUT

Public Meeting

Bicycle & Pedestrian Master Plan

Attend in person:
June 20, 2023
7:00 p.m.
Avon Senior Center
635 West Avon Road

Scan or click to learn more
from the Project StoryMap!

<https://arcg.is/0851im>

Town of Avon, Connecticut <https://www.avonct.gov/subscribe>

Town Council Meeting welcome and call to order – Dan Polhamus, Avon Town Council Chair

Presentation – Holly Parker & Fiona Flynn - SLR

The presentation began with a discussion about the progress the Town of Avon has been making toward creating policy and culture change that support biking and walking, and about the compromises that will have to be made to accommodate those modes given the town's terrain and other factors that revealed themselves when assessing the feasibility of adding bike lanes, sidewalks, and other facilities to Avon's roads.

Research, analysis of public comments, and work with the Advisory Team including staff from Avon's Planning and Community Development Office, Town Manager's office, Public Works, Recreation and Parks, and the Police Department have all contributed to an understanding of the opportunities and constraints to improving Avon's roads for walking and bicycling.

Meeting attendees were reminded that the Project's Vision Statement explains the purpose of the study as being, "...to facilitate public health, recreational opportunities, and better air quality" so access and safety are improved for all who use the road and trail network. Holly Parker of SLR Consulting further explained that the presentation would focus primarily on how decisions about new investments in bicycle and pedestrian infrastructure and amenities can be guided.

The presentation's agenda and project timeline were shown, and the next slides presented the results of the first three "Mobility Questions of the Month." A fourth question will remain open and posted on the project's StoryMap website until the end of June. The June question asks the degree to which Avon residents are satisfied with the bus system, road safety, bicycle accommodation on the roads (e.g., sharrows, wider shoulders, narrower travel lanes), bicycle parking in Avon, and pedestrian safety (e.g., wider shoulders, more sidewalks, or more speed enforcement), and attendees were encouraged to answer the question, and ask family and friends to do so as well.

The existing conditions on Avon's roads that inform possible recommendations for bicycle and pedestrian facilities were discussed next. The posted (and actual) speed on the town's roads, survivability of crashes relative to speed, width of lanes and shoulders, and amount of available Right of Way (ROW) will decide how feasible bike lanes, sidewalks and other treatments will be. The fact that there's a significant differential in speed among different transportation modes was discussed: walking speed is 3 to 3 ½ miles per hour, biking speed is 10 to 12, and driving is 25 or more miles per hour. This is what makes us feel unsafe when we aren't driving.

Next, a map of the available Public ROW on Avon's major north/south and east/west roads was shown – depicting the degree to which the amount of area available for sidewalks or sidepaths varies. The public ROW was defined as the paved portion of the street, as well as the area outside of the pavement required to accommodate traffic signs, utilities, sidewalks, and is owned by either the Town or the State. The location where a homeowner's property meets the Town's ROW is often designated with a pin or a monument.

Avon's progress at creating a policy framework and programming that supports bicycle and pedestrian infrastructure and culture changes was discussed next. The Town Council passed a Complete Streets Resolution on June 1st, and the fact that the speed limit on the Farmington Canal Trail is 12 miles per hour is also a point of policy as well as an opportunity for education. Another point of policy is that the town's zoning is currently being reviewed for changes – which is an opportunity to include such elements as bike parking and/or sidewalks as conditions of new development. From the programming perspective, the town is now officially a Bicycle Friendly Community – having received that designation from the League of American Bicyclists at the end of May. Avon has also hosted a bike rodeo, trail clean up day, group bike rides and hikes, bike to work and school days, and used bicycle collections throughout the year. A Bicycle Education program for fourth graders attending the Avon Public Schools began this year.

The next slides showed roadway characteristics and constraints on Route 44 and Country Club Road – which are among those proposed for primary bicycle connectivity. The other roads proposed to be in the Primary Connectivity Network are: Nod Road, Route 10/Waterville Road, Scoville Road, Route 167/West Avon Road, and Route 177/Lovely Street. Slides showing the characteristics and constraints of example roads Juniper Drive and Burnham Road – within the Secondary Connectivity Network – were presented as well. The other roads in the proposed Secondary Connectivity Network are Arch Road, Woodmont Road, Hollister Drive, Chevas Road, and Huckleberry Hill Road. The Farmington Canal Trail is a critical part of the network connecting potential future bicycle facilities.

With all of the preceding as context, a Bicycle Connectivity Network map was presented showing how Primary and Secondary Connectivity Networks weave the town's destinations together. The PRIMARY network is a more regional system and is made up of state roads (with the exception of Country Club and Harris Roads) – connecting to adjacent towns, while the SECONDARY Connectivity Network is more for making local connections.

A future multiuse trail was featured on the Bicycle Connectivity Network map - part of the Old Farms Road Project's Phases I and 2 which will include the construction of a paved multi-use trail beginning on Scoville Road at the Farmington Canal Heritage Trail.

The next slide showed a map of the Pedestrian Connectivity Network, showing existing sidewalks in important destinations – including schools, grocery stores, and trailheads. A ¼ and ½ mile “walkshed” – meaning a buffer around the existing roads was created to show the opportunity to connect areas of Avon with sidewalks or sidepaths that radiate out from each of Avon's identified destinations. This would provide a slow but cost-effective approach to eventually connect the town with sidewalks and paths.

The proposed project on Country Club Road – the grant funding for which has been tentatively awarded – will complete the sidewalk from the Library east to the Fisher Meadows Recreation Area, proving a connection from the Center of Avon to an important recreation area and to the Farmington Canal Trail.

The next slides explained the decision-making criteria used to determine how best to proceed with bicycle and pedestrian facility planning. At the macro level, they include safety, feasibility, equity, connectivity, and cost. Further criteria for decisions include guidance from the Connecticut Department of Transportation (CTDOT) Highway Design Manual on lane widths, a particular roadway's Functional Classification, its amount of daily traffic (Annual Average Daily Traffic, or AADT), and its travel speeds.

Juniper and Burnham Roads both meet the CTDOT criteria for 10 foot lanes – narrower than they are now, and allowing for the addition or widening of the shoulders by painting fog lines. Burnham Road is being investigated as a possible candidate for narrower lanes and fog lines. Both of those roads are scheduled for repaving this year, providing a potential opportunity to try this treatment.

The Federal Highway Administration (FHWA)'s Bikeway Selection Guide shows how to determine the best types of bicycle facilities for a given roadway based on the volume and speed of car traffic.

Some of the specific “tools” in the bicycle facility planning “toolbox” include shared lane markings – with the caveat that they don't replace bike lanes – rather are a relatively cost-effective first step in raising driver and cyclist awareness that bicycles can and should use the car/truck travel lanes when the bicyclist feels comfortable doing so. Signage indicating that bicycles are on the road also creates this awareness and wayfinding signage is also an important means of encouraging bicycling by helping pedestrians and cyclists navigate to destinations.

The “tools” in the pedestrian facility planning “toolbox” include crosswalk visibility enhancements, raised crosswalks, pedestrian refuge islands, Rectangular Rapid Flashing Beacons (RRFBs), and curb extensions. Some of these treatments could be applied to Route 44 as well, but given its 4 lanes, traffic speeds and volumes, and wider ROW, other treatments are available and/or warranted.

The last segment of the presentation focused on the kinds of roadway configurations that accommodate bicyclists and pedestrians that are possible on Avon's roads, given their width and ROW, as well as the many other constraints mentioned previously. These were shown as cross sections depicting varying widths of vehicle travel lanes, bicycle lanes, shared lane markings, sidewalks, multi-use paths, and buffer strips between transportation modes. The example of Country Club Road was provided, and some of the configurations shown in the cross-section slides were superimposed onto Country Club Road to show what it would potentially look like if it had 1) only conventional sidewalk, shared lane markings, and a “Bikes May Use Full Lane” sign; 2) a 10-12 foot divided shared use path that accommodates bicycles and pedestrians, and possible signage indicating bicycles use the left side and pedestrians use the right; and 3) on-street bicycle lanes separated by a painted buffer strip and sidewalk. Participants were asked to vote on the three options, and overwhelmingly chose option 2.

Next steps include the development of defined bicycle and pedestrian connectivity opportunities, consideration of new input from this meeting, identification of funding opportunities, ballpark cost estimates, and preparation of an implementation matrix. As a last step, the Bicycle and Pedestrian Master Plan will be presented to the Town Council for approval.

Comments for additional consideration in next steps include:

- Trailblazing guidance/signage for Farmington Canal Trail (esp. from Sperry Park to Whole Foods)
- The effect of narrowing lanes on general safety
- Consideration of Climax Road as part of the Secondary Bike Network
- Wayfinding
- Review Bike Plan from 10 years ago (reference to map created by Avon native Chris McCahill, which is included in Avon's Plan of Conservation and Development)
- Connection to Unionville
- Wetlands as a constraint
- Stagecoach Road for a North/South connection

Third Public Meeting | September 19, 2023 | Avon Bicycle and Pedestrian Master Plan | 3rd Public Meeting | Avon Senior Center | Meeting Notes

Attendees

Town of Avon

Brandon Robertson – Town Manager

Grace Tiezzi – Assistant Town Manager

Hiram Peck – Director of Planning and Community Development

Avon Town Council Members

Dan Polhamus, Chairman

Heather Maguire

Barbara Ausiello

Erin Barthel

Anthony Weber

SLR

Holly Parker – Senior Transportation Planner/Project Manager

Dave Sullivan – US Manager, Transportation Planning

Town Council Meeting welcome and call to order – Dan Polhamus, Avon Town Council Chair

Presentation – Holly Parker & David Sullivan - SLR

The presentation began with an appreciation of the collaboration and partnership the Town has had with SLR, and its' responsiveness to the Avon community – which requested this third public meeting so that the proposed recommendations for bicycle and pedestrian infrastructure, policy, and signage could be shared before presentation to the Town Council.

Early in the presentation, a group of High School and Middle School students, the “Green Valley Volunteers” led by M. Gokalp Gokcen, came to the podium to share their vision to make Avon’s roads safer for walking and bicycling – especially Lovely Street (Route 167).

Most of the presentation focused on the specific recommendations made for pedestrian and bicycle infrastructure improvements, including the process for developing infrastructure recommendations. The presentation finished with the policy, program, and signage recommendations that could improve safety and behavior, and begin to change the culture for road users regardless of travel mode – such as a reinvented “gateway” sign welcoming visitors to Avon’s “Walkable + Bikeable Community,” and reminding drivers to do so carefully.

Comments for additional consideration included:

- Separated bike lanes on Lovely Street (Green Valley Volunteers)
- There are no sharrows recommended
- Connect neighborhoods on the east side of Route 10 to each other, to Fisher Meadows, and to the Magnet School
- There should be a consistent design element
- Extend West Avon Road pedestrian improvements shown on Section 14 around the Roaring Brook School further north



The flyer features the Avon Connecticut logo and the title "Public Meeting Bicycle & Pedestrian Master Plan". It provides details for an in-person meeting on September 19, 2023, at 7:00 p.m. at the Avon Senior Center, 635 West Avon Road. A QR code is provided for more information, along with the URL <https://arcg.is/0851im>. A photograph shows a "SLOW DOWN Avon" sign in front of a house. A note states: "Slow Down, Avon" signs can be purchased at Dom's Coffee Shop (20 West Main Street). All proceeds will be donated to Gifts of Love in Avon. The footer includes "Town of Avon, Connecticut" and the website <https://www.avonct.gov/subscribe>.



Appendix C Pedestrian and Bicycle Concepts Maps

Bicycle and Pedestrian Master Plan

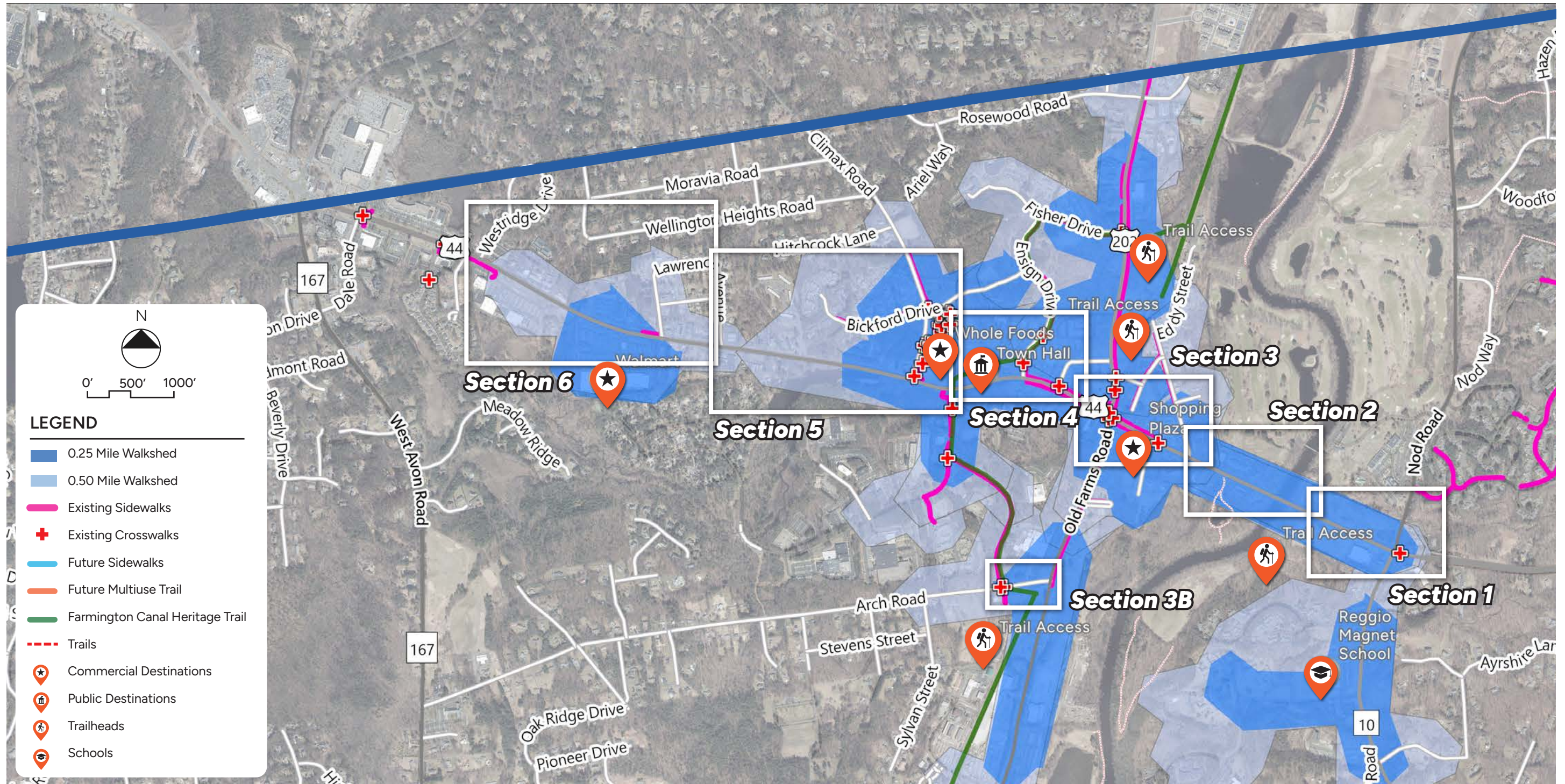
Town of Avon

SLR Project No.: 141.13130.00016

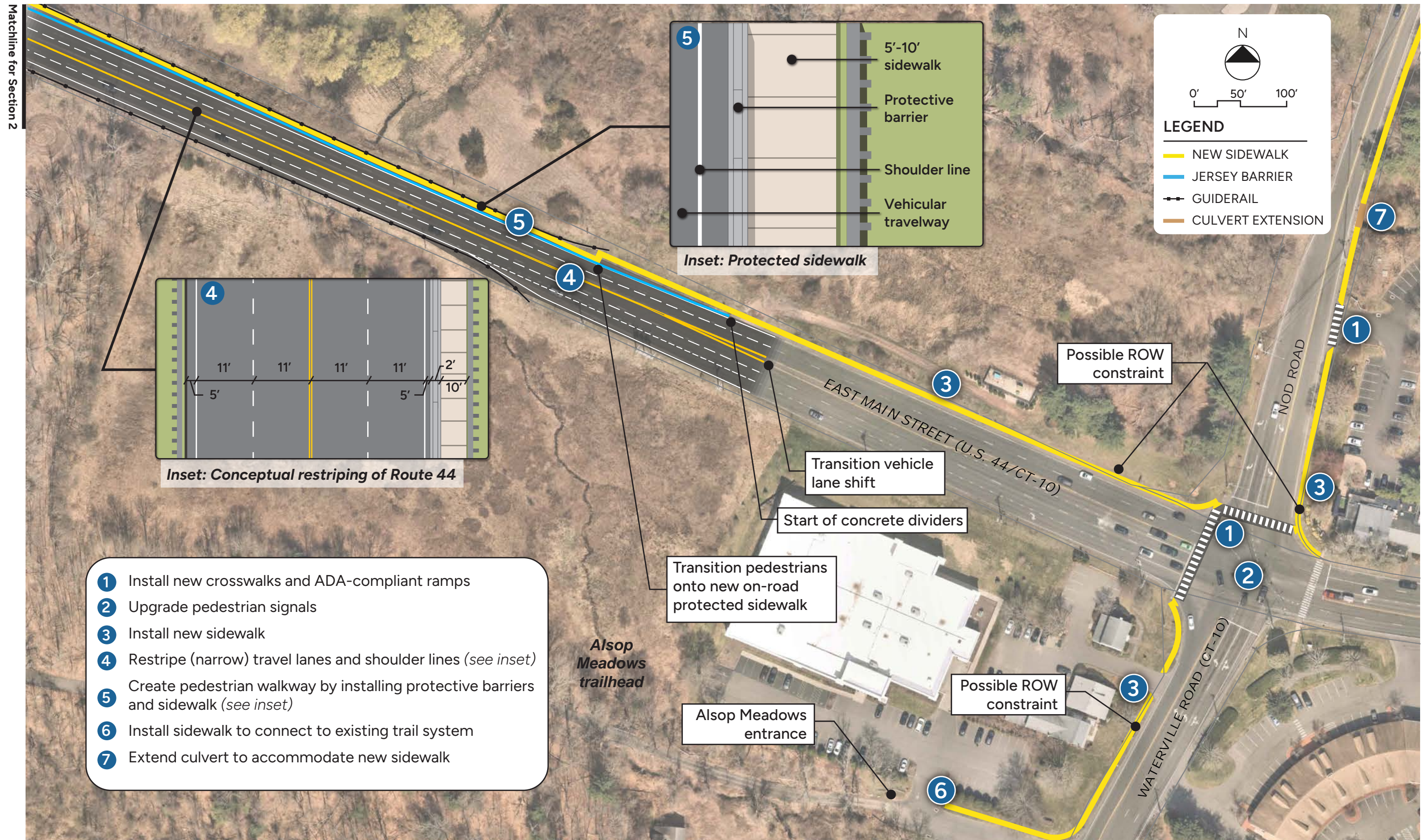
February 29, 2024



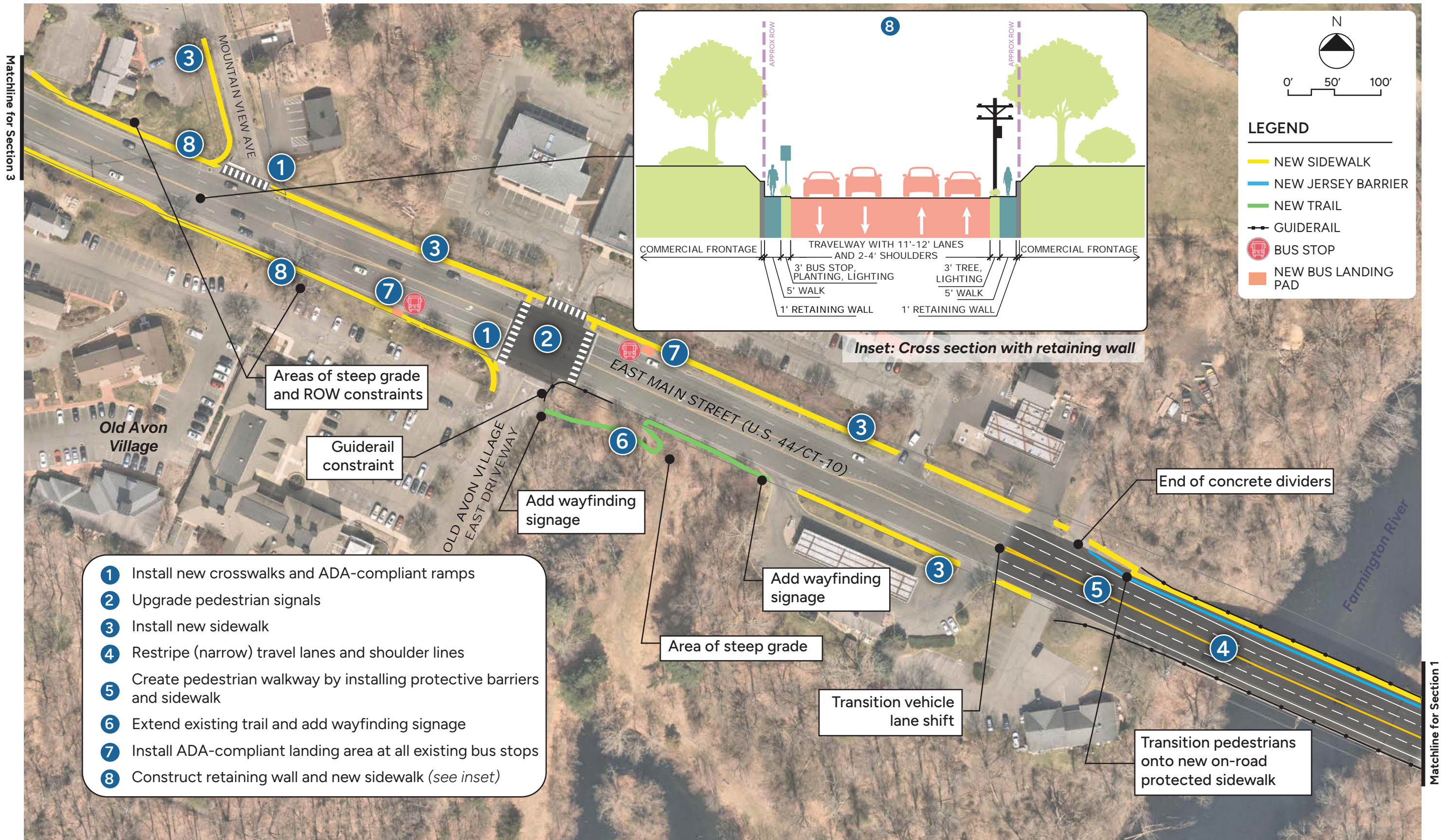
WEST MAIN STREET (U.S. 44) WALKSHED INDEX MAP



WEST MAIN STREET (U.S. 44) SECTION 1: NEAR NOD ROAD

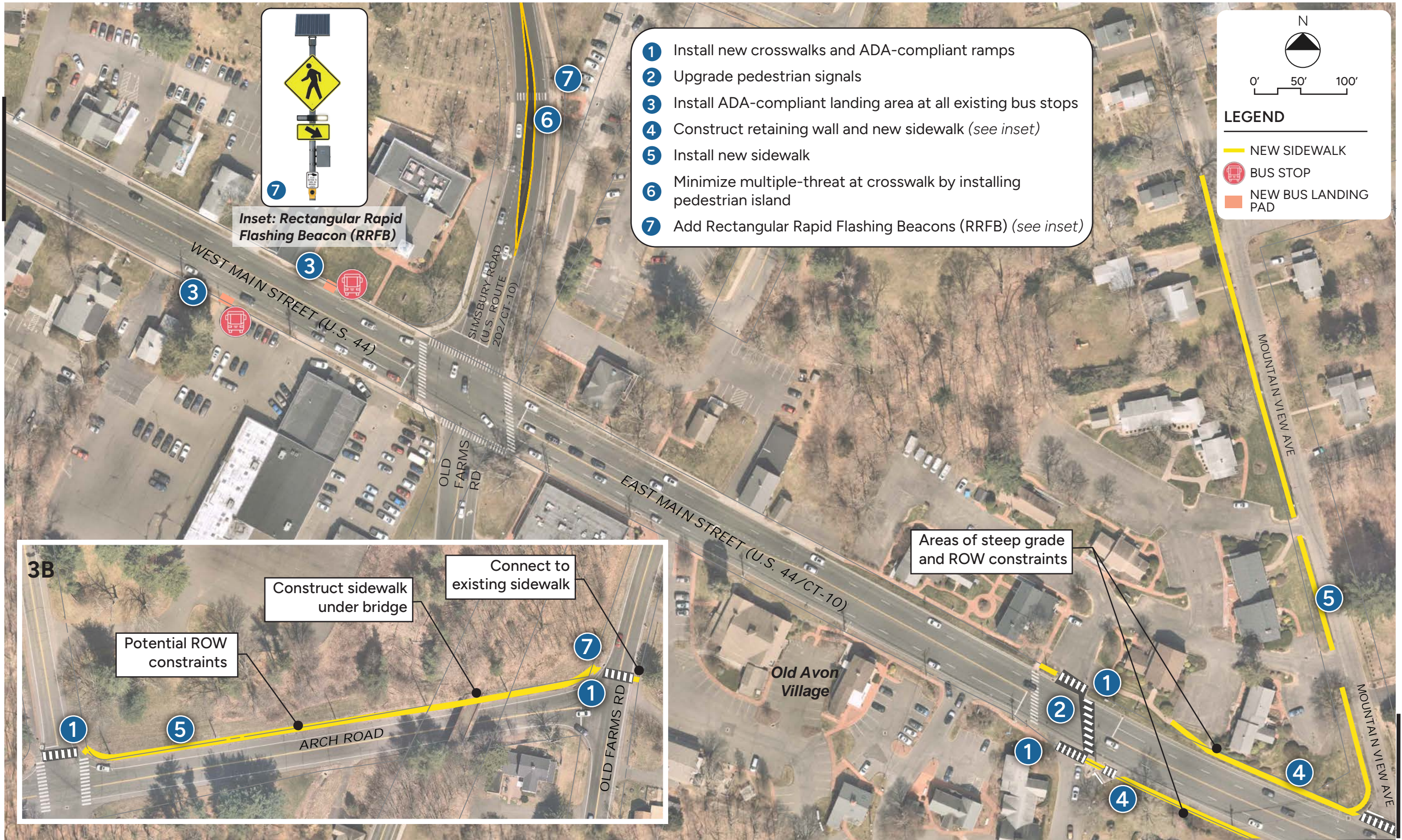


WEST MAIN STREET (U.S. 44) SECTION 2: NEAR FARMINGTON RIVER AND OLD AVON VILLAGE



WEST MAIN STREET (U.S. 44) SECTION 3: NEAR OLD FARM ROAD/SIMSBURY ROAD (U.S. ROUTE 202)

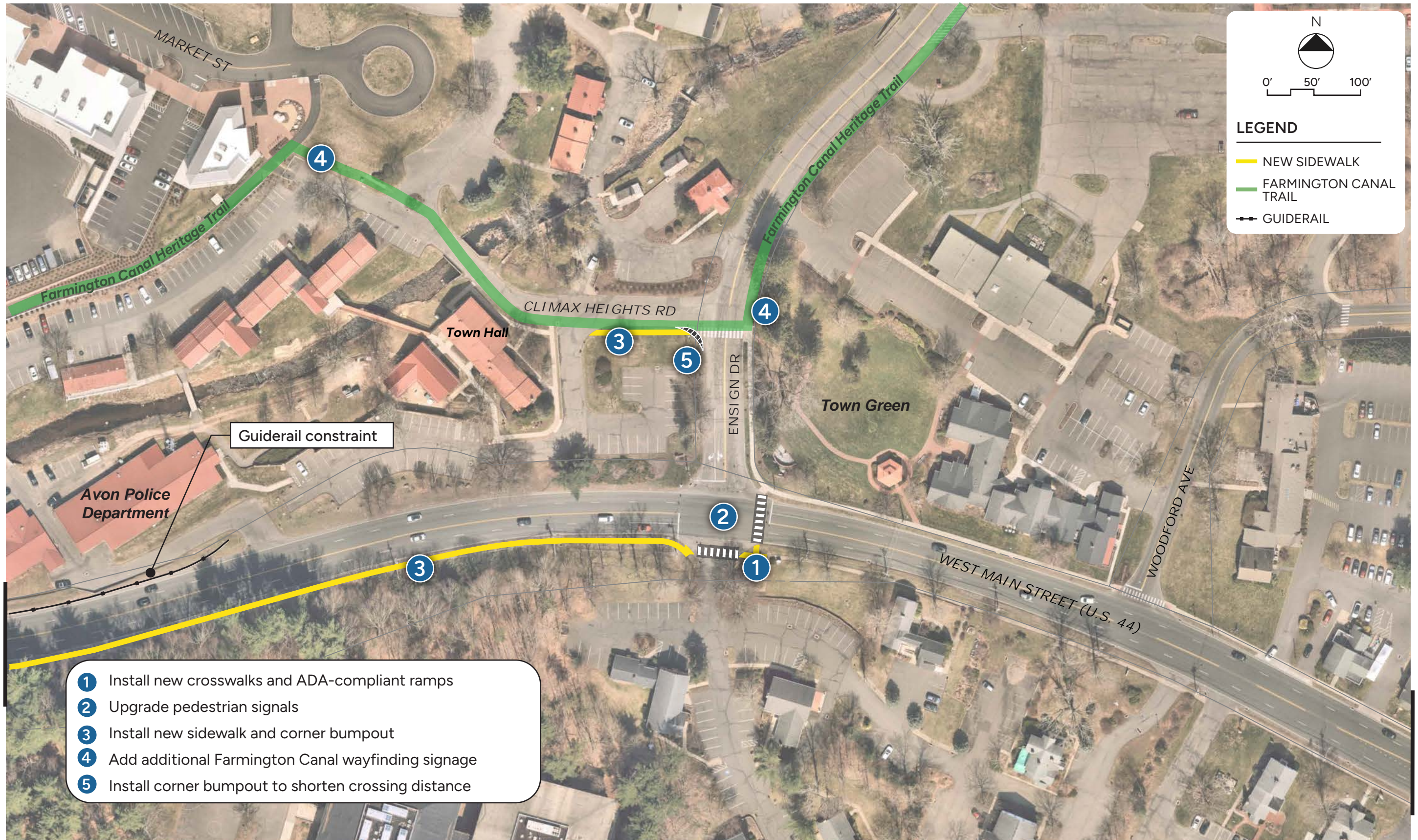
Matchline for Section 4



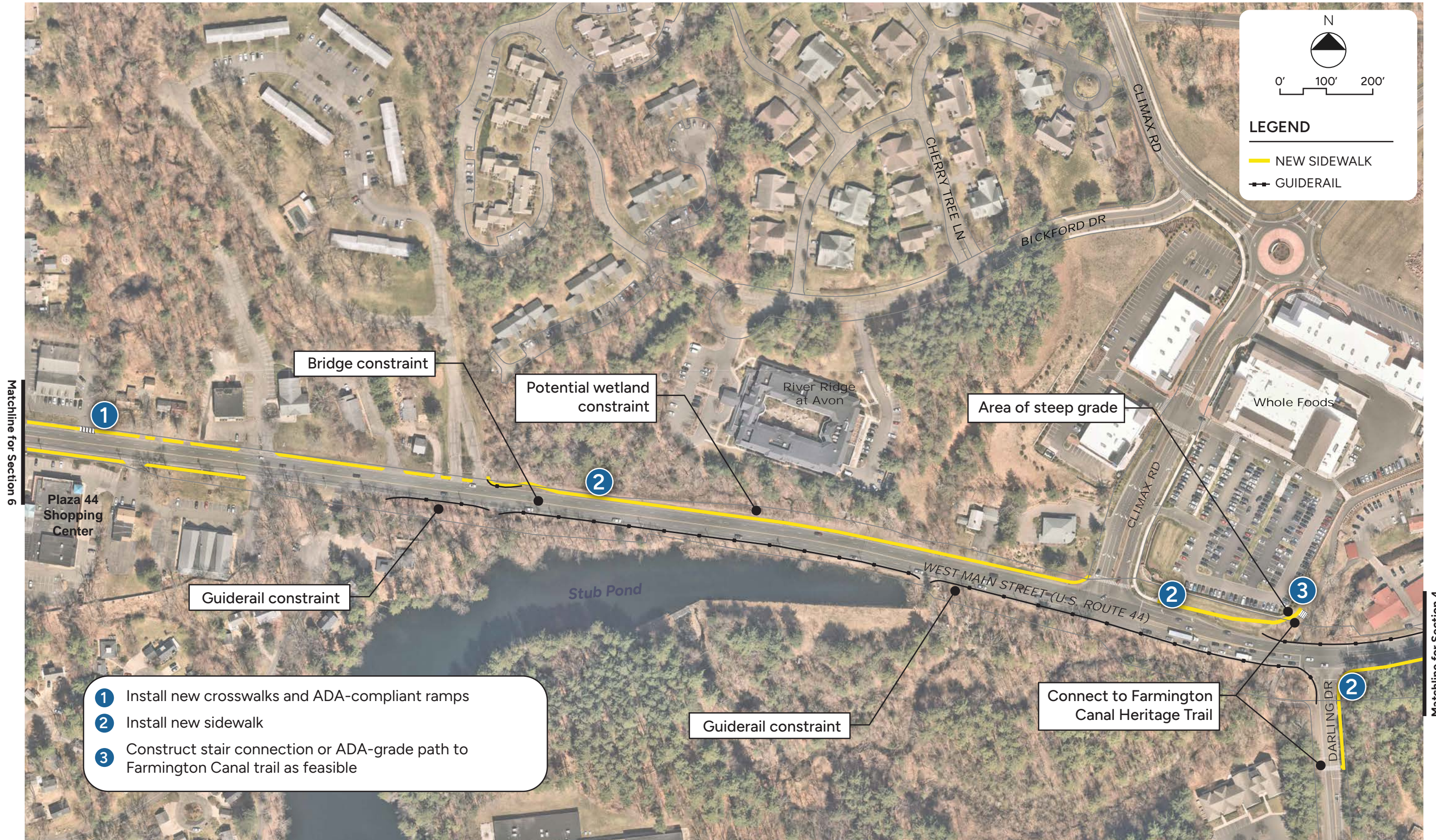
Matchline for Section 2



WEST MAIN STREET (U.S. 44) SECTION 4: AT ENSIGN DRIVE, NEAR TOWN HALL



WEST MAIN STREET (U.S. 44) SECTION 5: NEAR WHOLE FOODS AND STUB POND



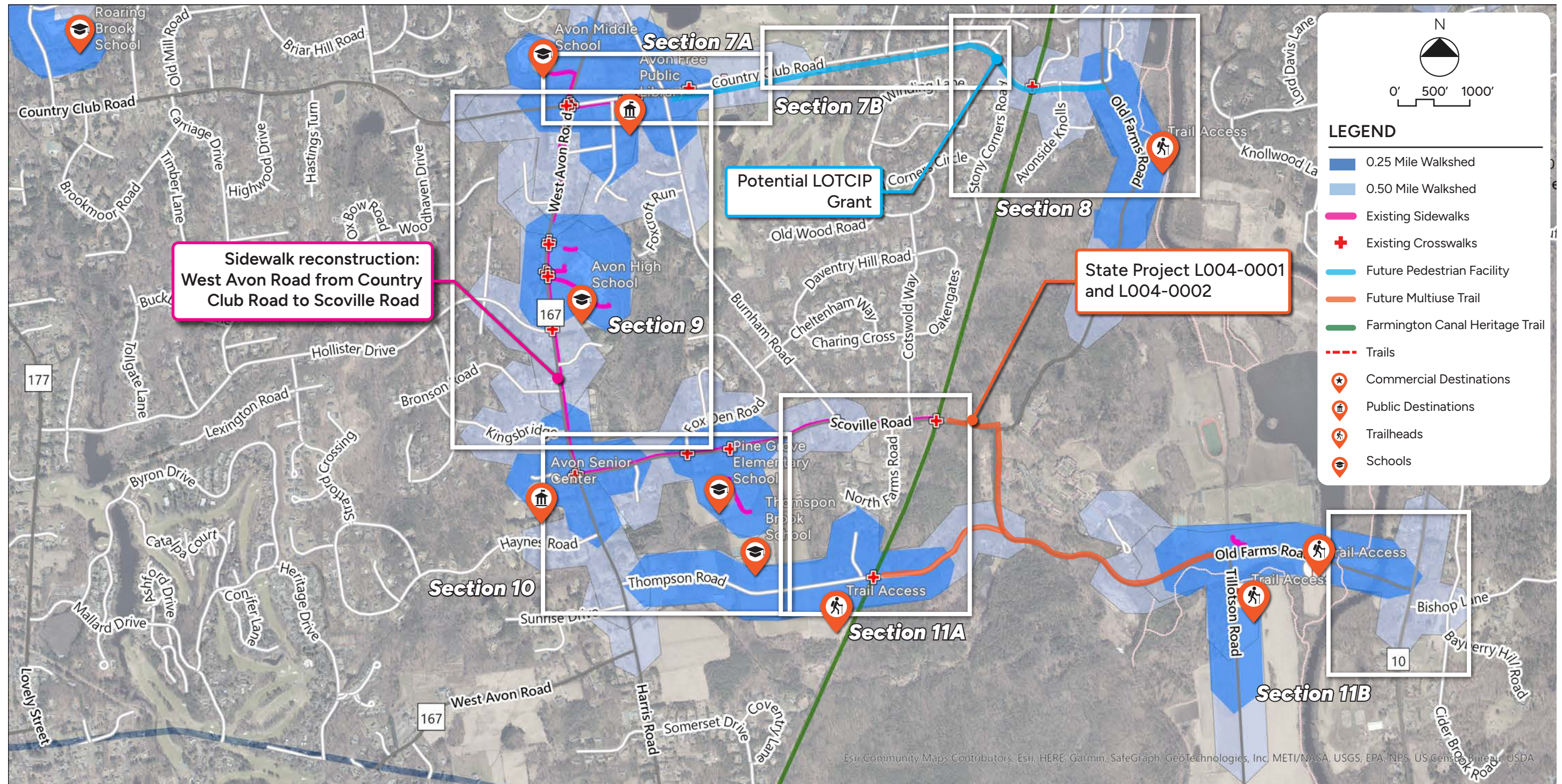
- 1 Install new crosswalks and ADA-compliant ramps
- 2 Install new sidewalk
- 3 Construct stair connection or ADA-grade path to Farmington Canal trail as feasible



WEST MAIN STREET (U.S. 44) SECTION 6: NEAR WALMART AND NOD BROOK MALL



CENTRAL AVON WALKSHED INDEX MAP



CENTRAL AVON SECTION 7: COUNTRY CLUB ROAD



Matchline for Section 9

Matchline for Section 7B



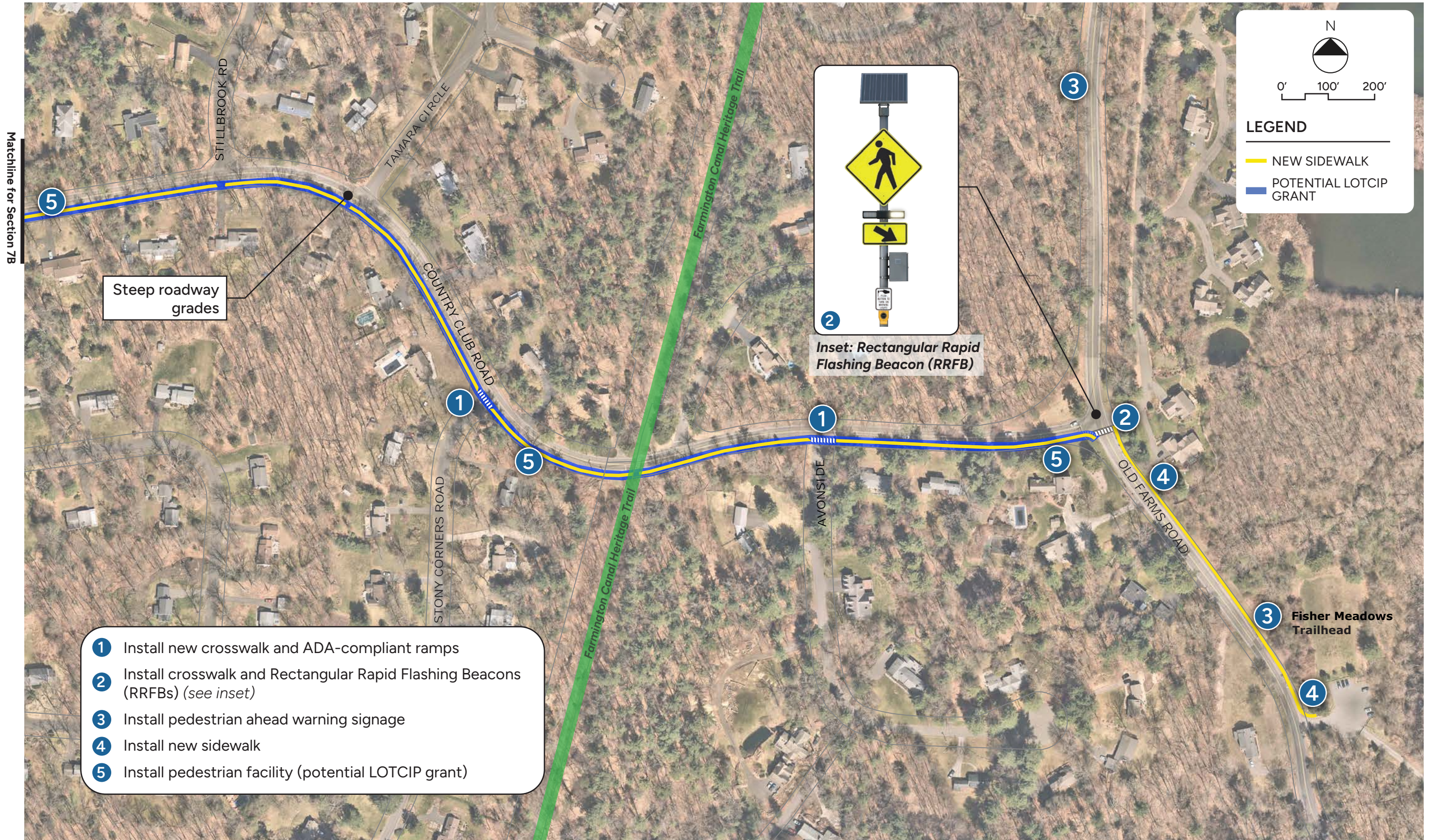
Matchline for Section 7A

Matchline for Section 8

- 1 Install new crosswalks and ADA-compliant ramps
- 2 Install pedestrian facility (potential LOTCIP grant)
- 3 Potential intersection art/treatment
- 4 Upgrade existing sidewalk



CENTRAL AVON SECTION 8: COUNTRY CLUB ROAD, NEAR OLD FARMS ROAD



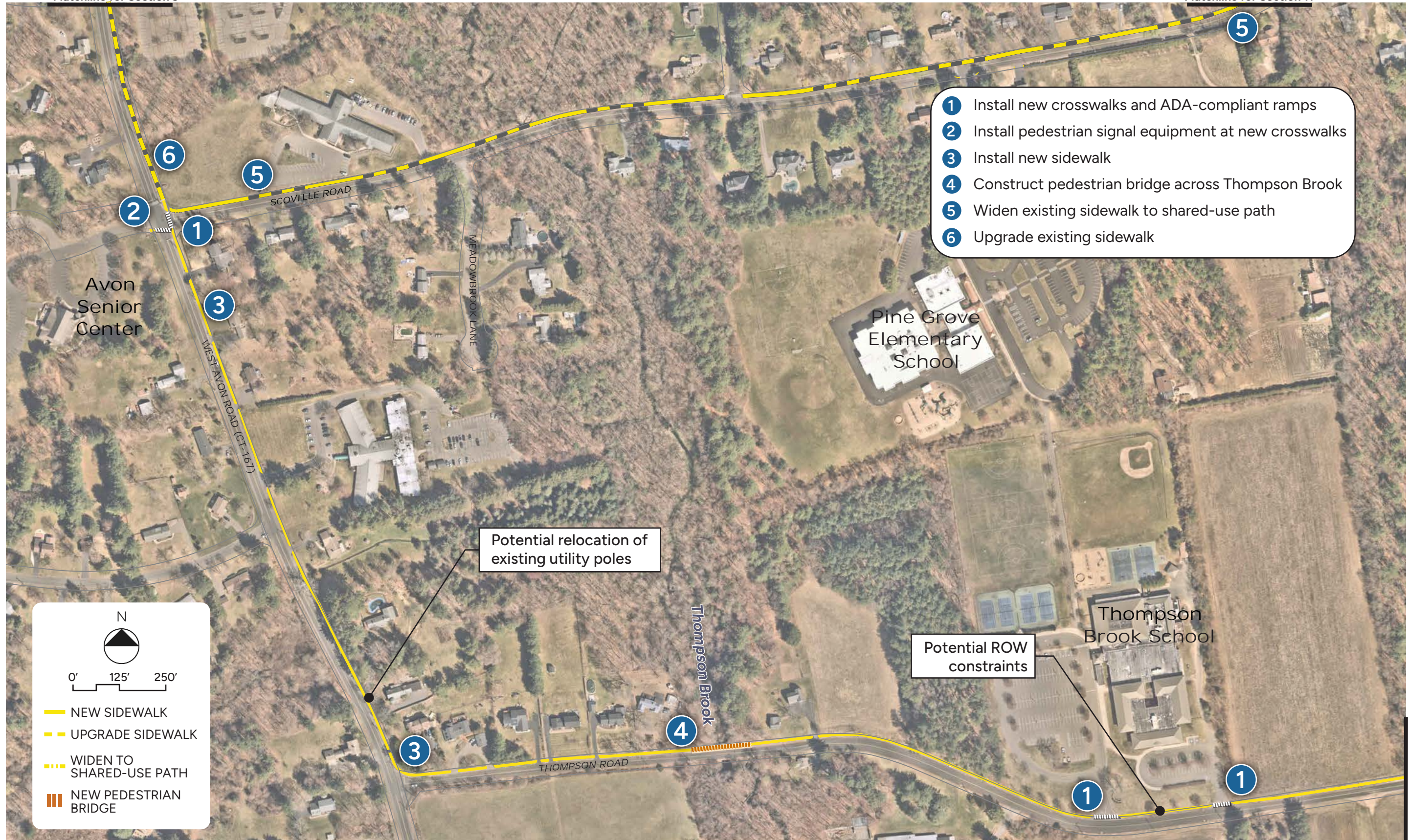
CENTRAL AVON SECTION 9: WEST AVON ROAD BETWEEN COUNTRY CLUB ROAD AND SCOVILLE ROAD



CENTRAL AVON SECTION 10: SCOVILLE ROAD AND THOMPSON ROAD, NEAR WEST AVON ROAD

Matchline for Section 9

Matchline for Section 11



- 1 Install new crosswalks and ADA-compliant ramps
- 2 Install pedestrian signal equipment at new crosswalks
- 3 Install new sidewalk
- 4 Construct pedestrian bridge across Thompson Brook
- 5 Widen existing sidewalk to shared-use path
- 6 Upgrade existing sidewalk

N

0' 125' 250'

- NEW SIDEWALK
- - - UPGRADE SIDEWALK
- ... WIDEN TO SHARED-USE PATH
- ||| NEW PEDESTRIAN BRIDGE

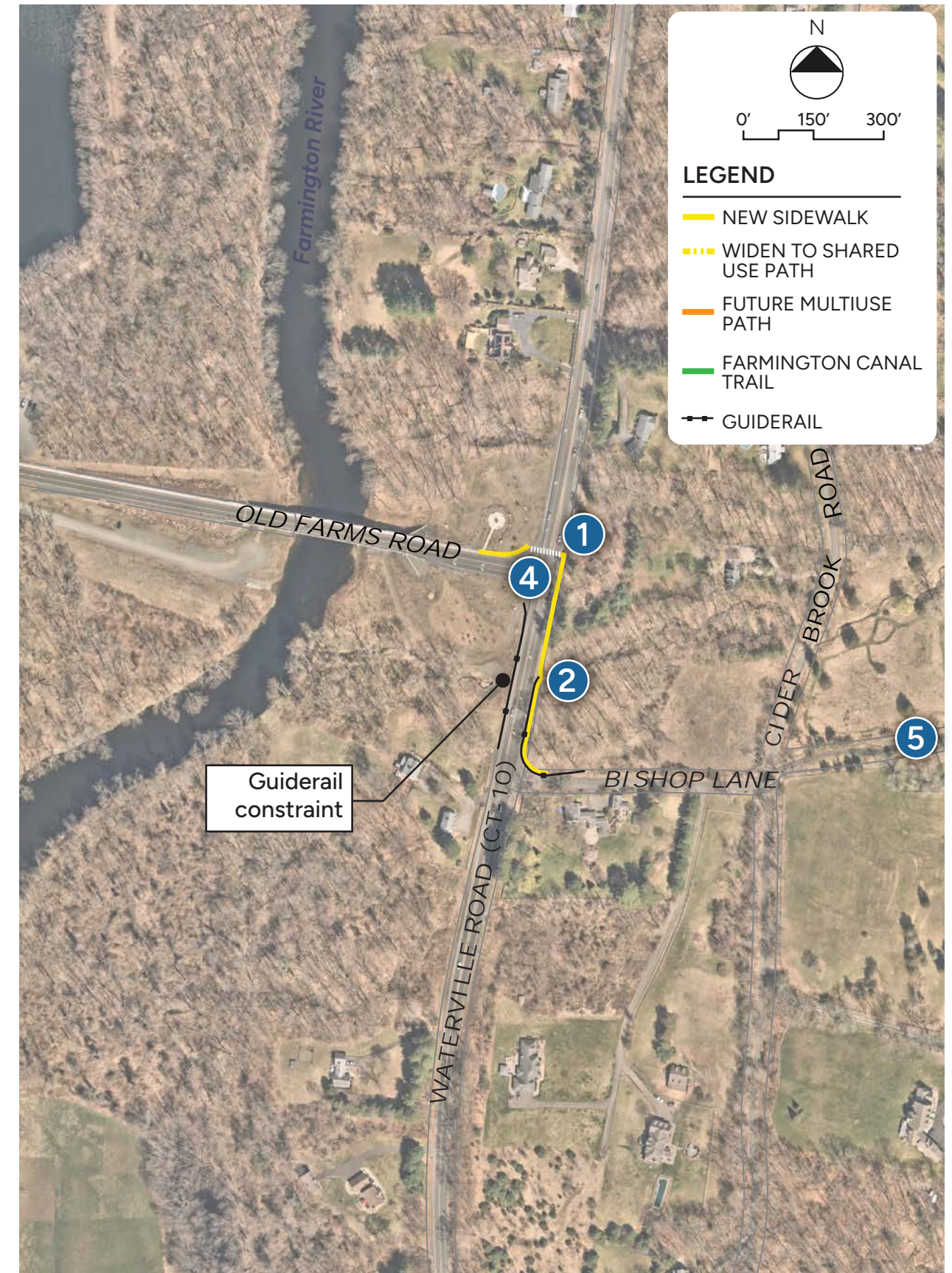
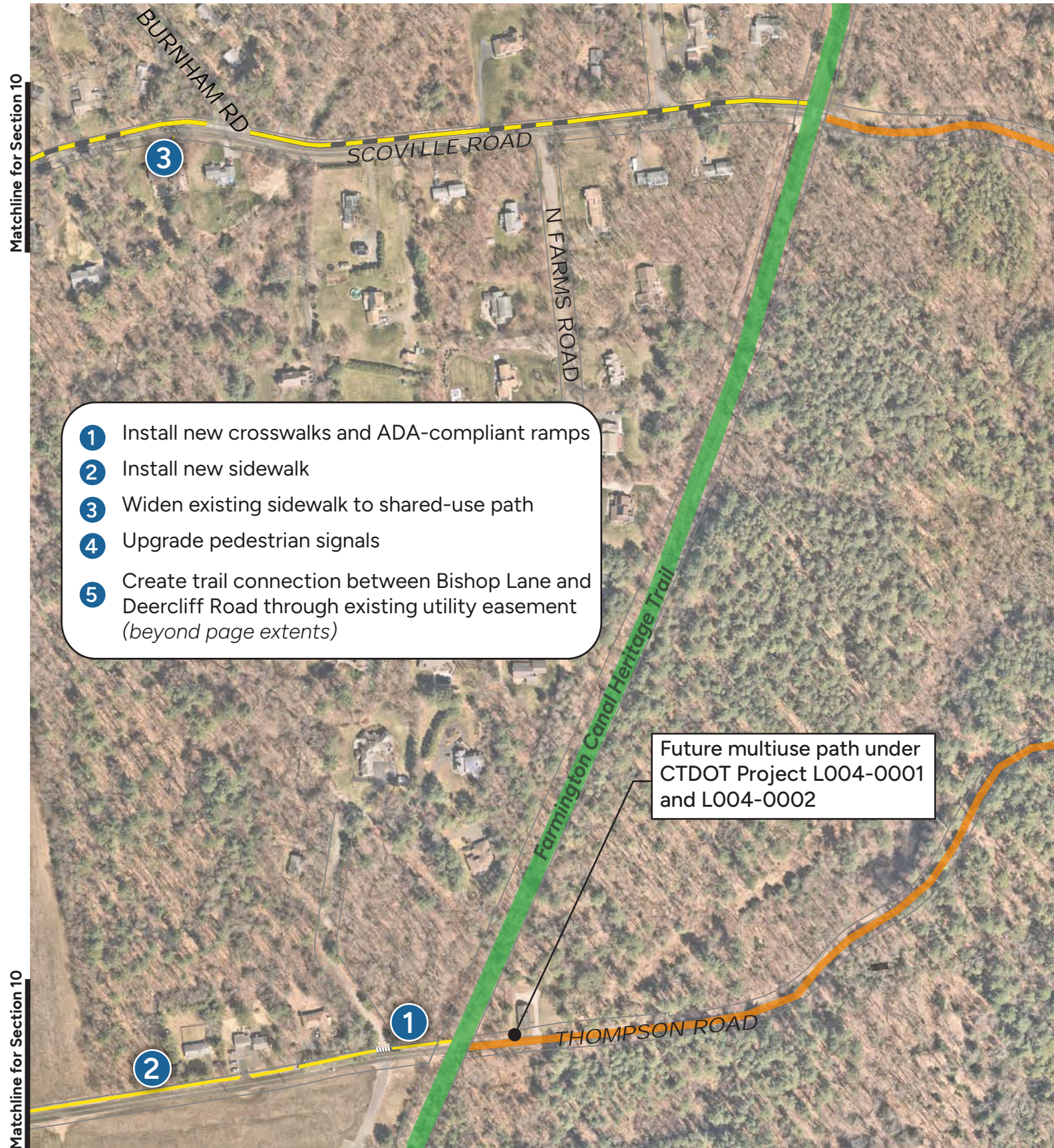
Potential relocation of existing utility poles

Potential ROW constraints

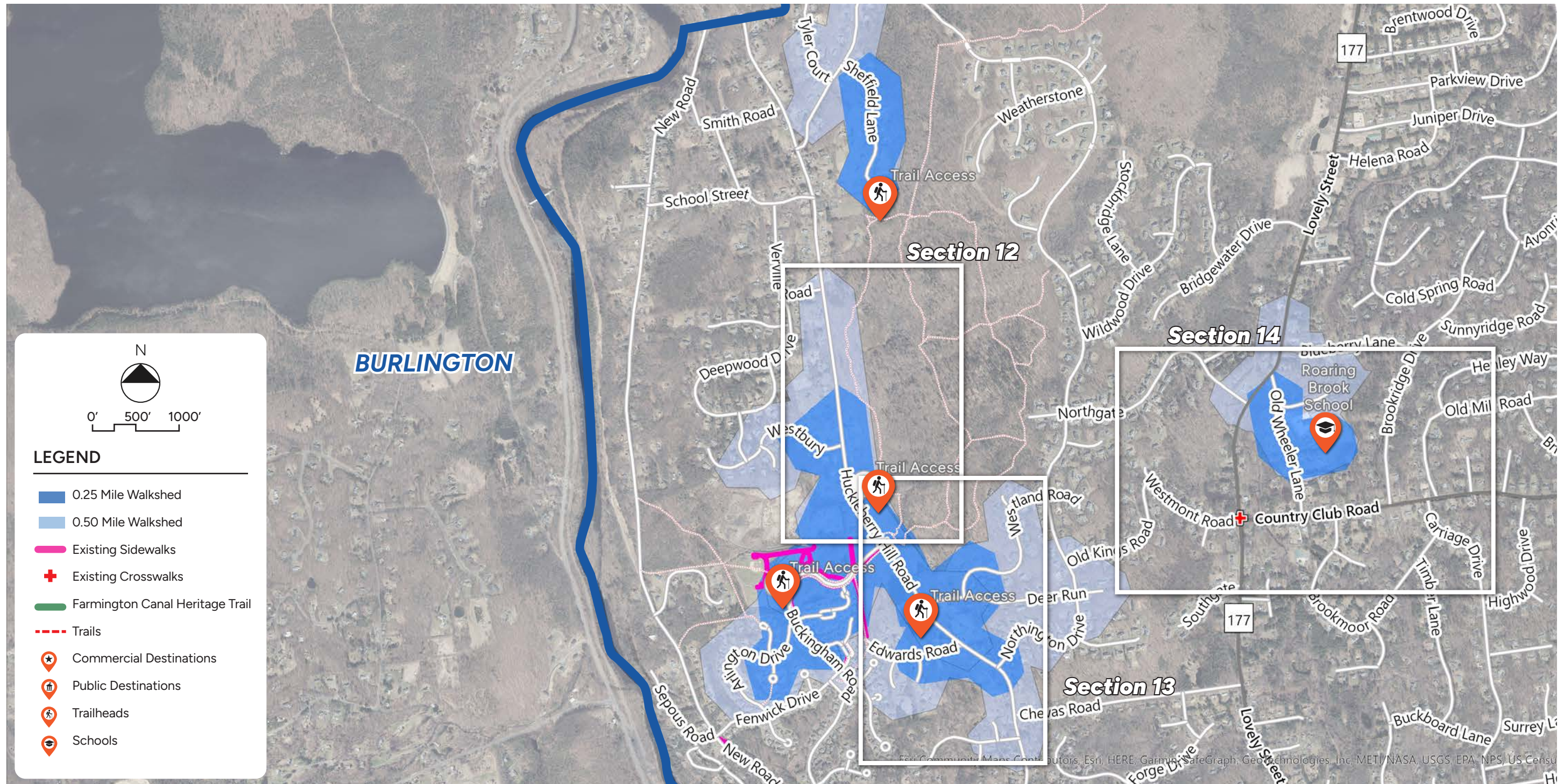
Matchline for Section 11



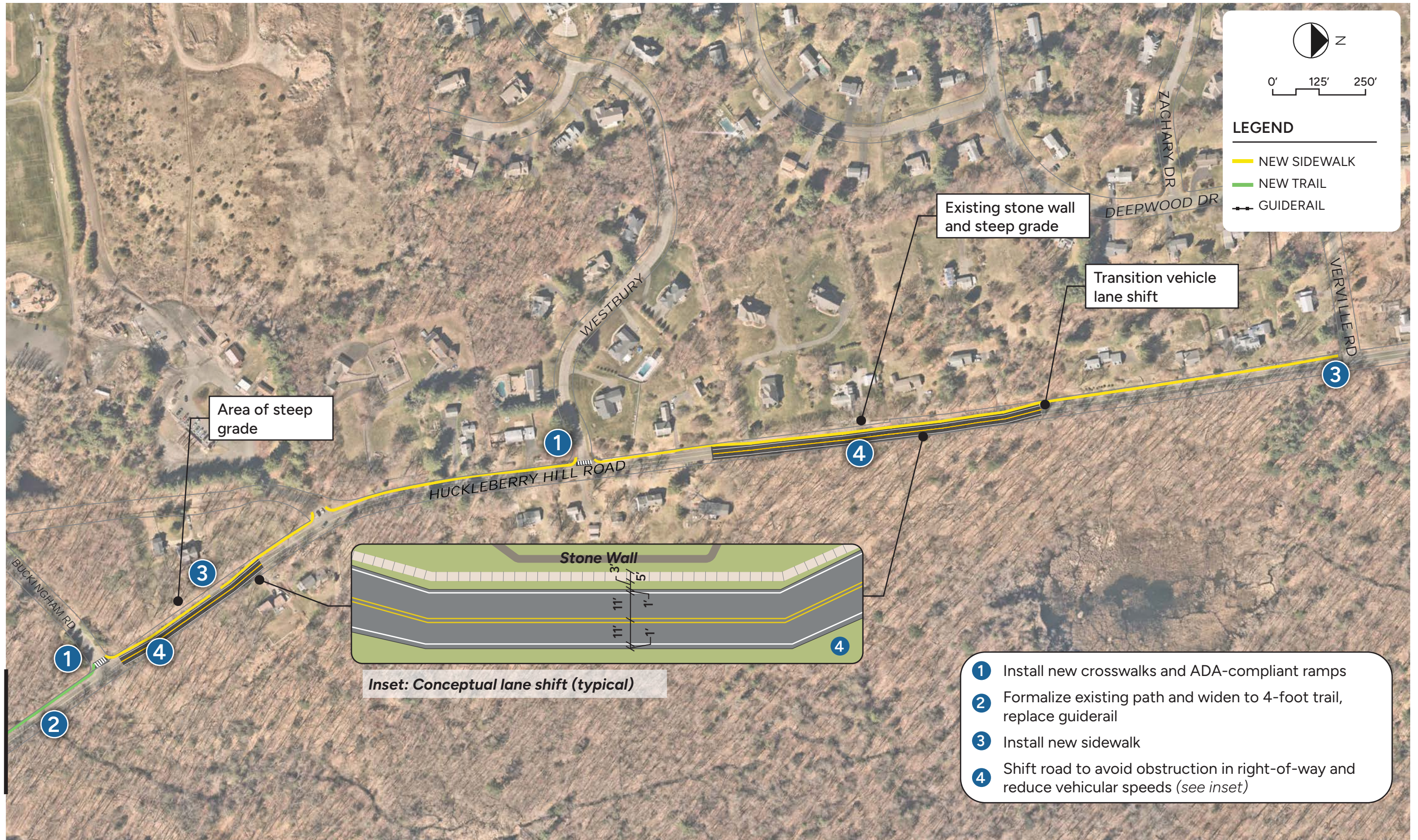
CENTRAL AVON SECTION 11: SCOVILLE ROAD, THOMPSON ROAD, AND OLD FARMS ROAD NEAR FARMINGTON CANAL



WEST AVON WALKSHED INDEX MAP



WEST AVON SECTION 12: HUCKLEBERRY HILL ROAD

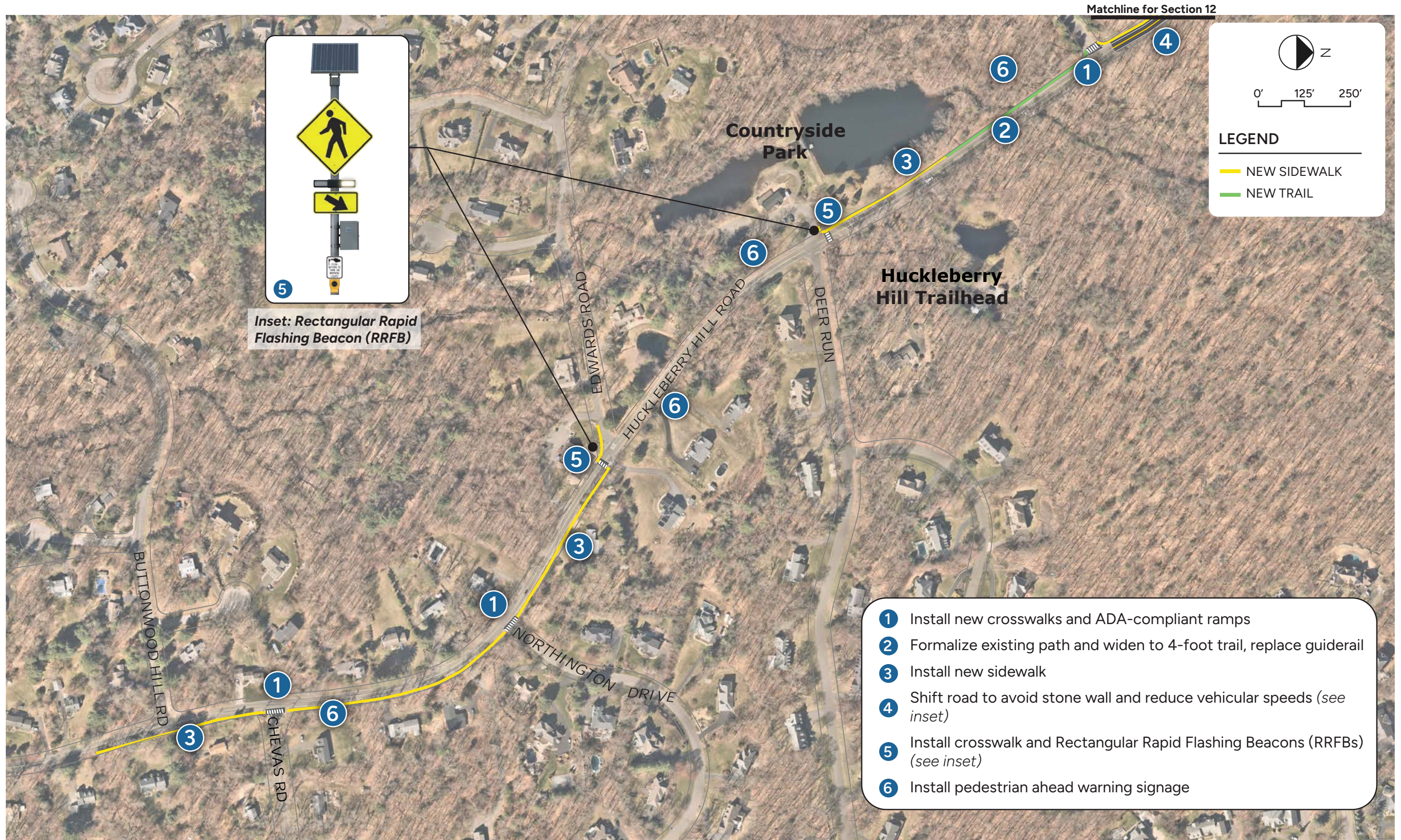


Matchline for Section 13

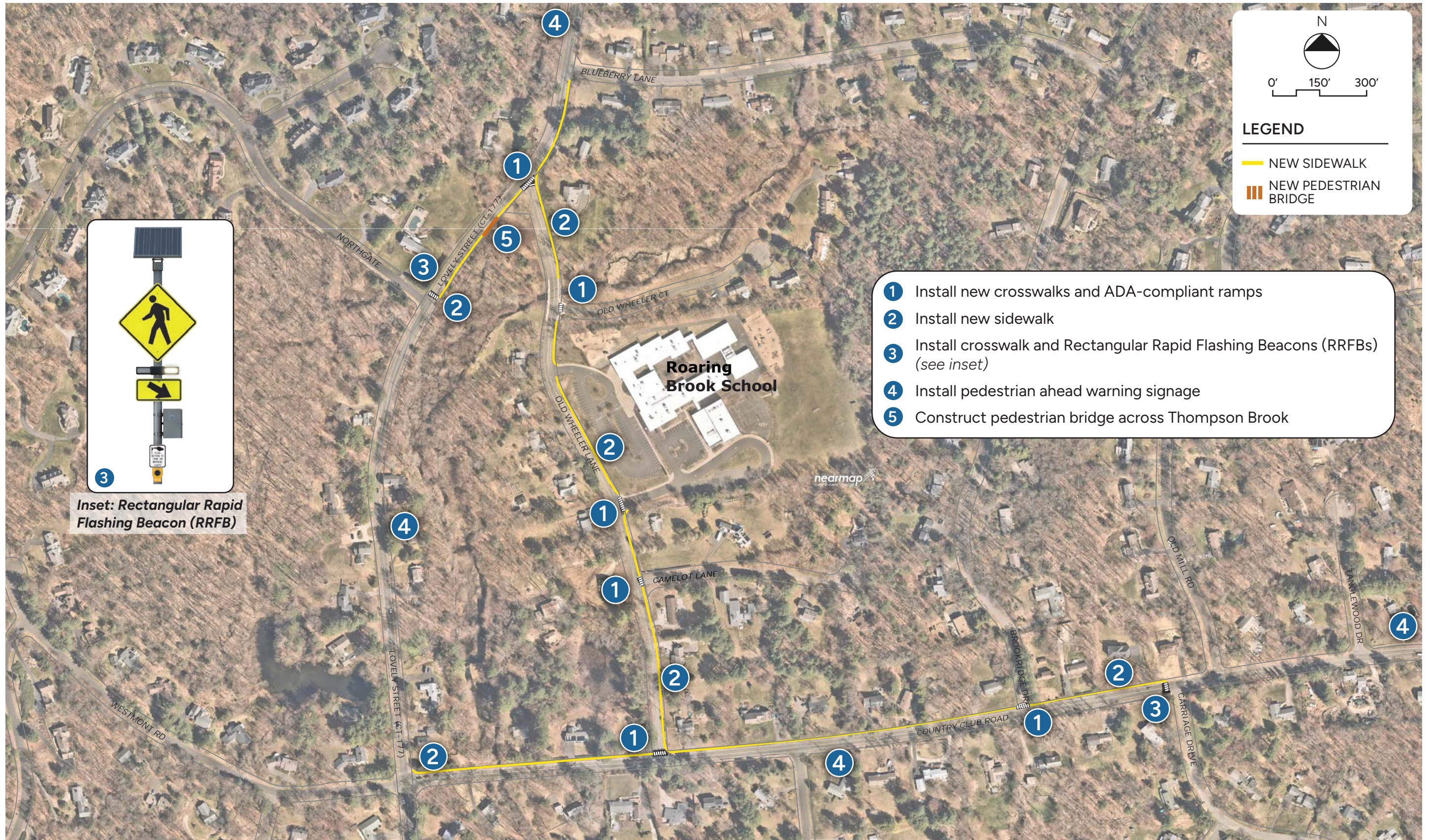
- 1 Install new crosswalks and ADA-compliant ramps
- 2 Formalize existing path and widen to 4-foot trail, replace guiderail
- 3 Install new sidewalk
- 4 Shift road to avoid obstruction in right-of-way and reduce vehicular speeds (see inset)



WEST AVON SECTION 13: HUCKLEBERRY HILL ROAD



WEST AVON SECTION 14: ROARING BROOK SCHOOL



TOWN OF AVON BICYCLE IMPROVEMENTS INDEX MAP

WEST Section












NORTHEAST Section

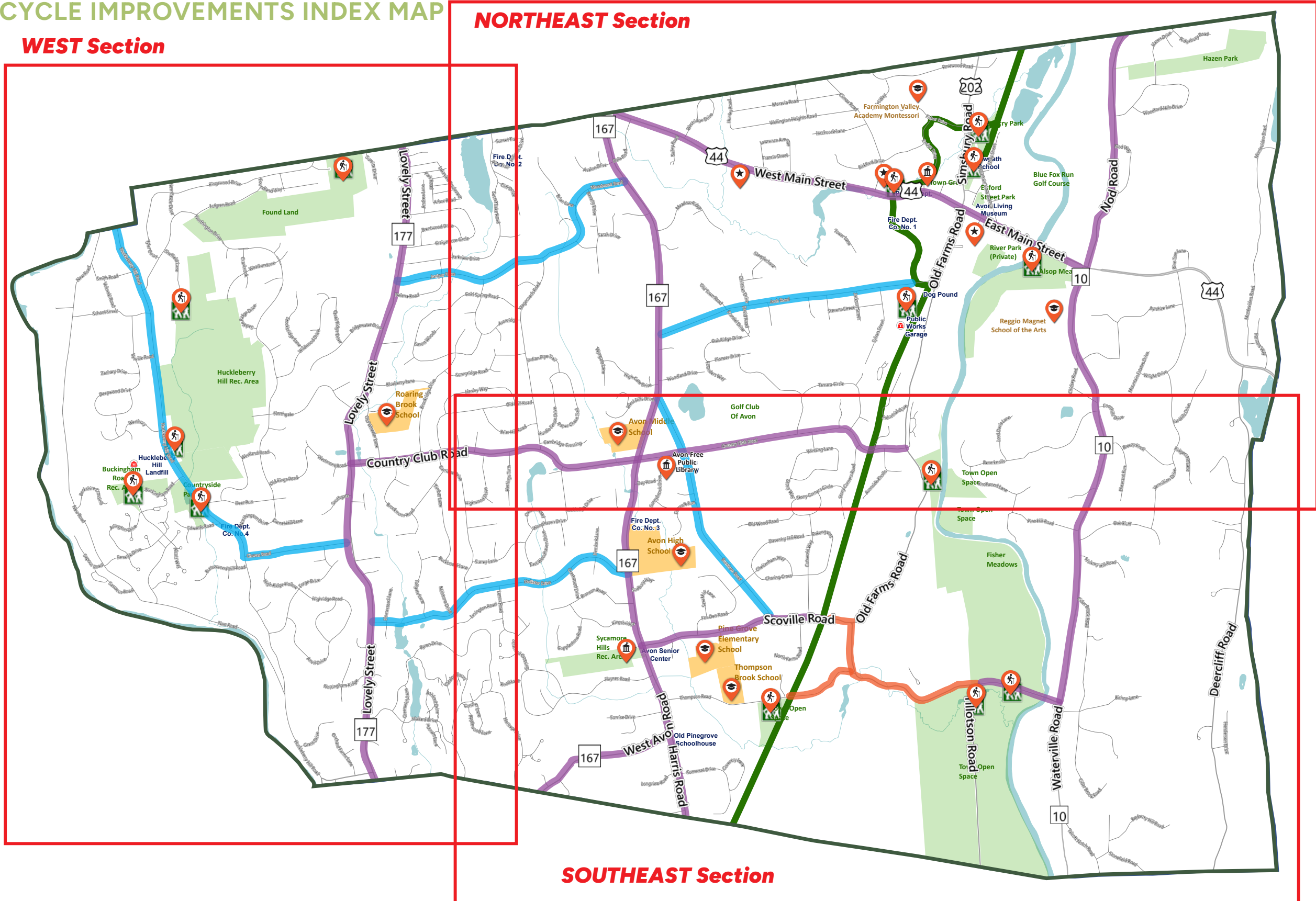
SOUTHEAST Section

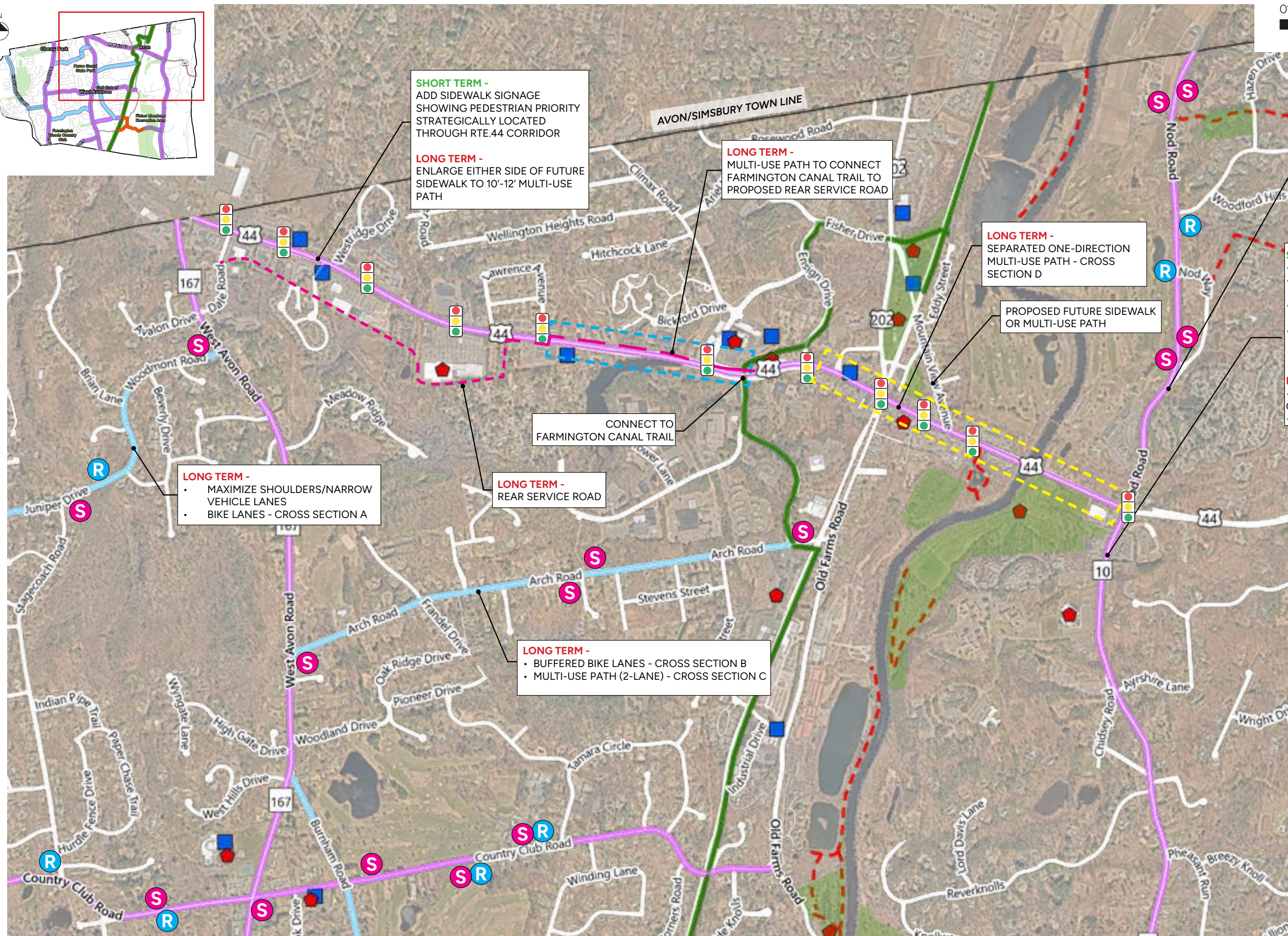
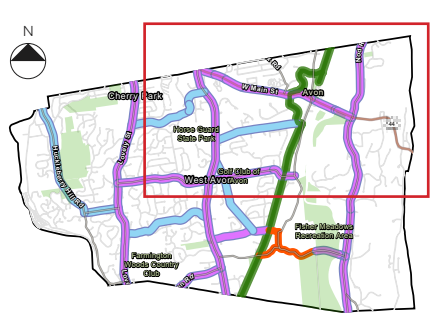


0' 1,500' 3,000'

LEGEND

-  Proposed Primary Connectivity Network
-  Proposed Secondary Connectivity
-  Future Multiuse Trail
-  Farmington Canal Heritage Trail
-  Trailheads
-  Public Schools
-  Points of Interest
-  Commercial Destinations
-  Public Destinations
-  Trailheads
-  Schools





SHORT TERM -
 ADD SIDEWALK SIGNAGE SHOWING PEDESTRIAN PRIORITY STRATEGICALLY LOCATED THROUGH RTE.44 CORRIDOR

LONG TERM -
 ENLARGE EITHER SIDE OF FUTURE SIDEWALK TO 10'-12' MULTI-USE PATH

LONG TERM -
 MULTI-USE PATH TO CONNECT FARMINGTON CANAL TRAIL TO PROPOSED REAR SERVICE ROAD

LONG TERM -
 SEPARATED ONE-DIRECTION MULTI-USE PATH - CROSS SECTION D

SHORT TERM -
 • MAXIMIZE SHOULDERS/NARROW VEHICLE LANES THROUGH TOWN REPAVING/ MAINTENANCE
 • PROVIDE BIKE LANES FOR LONG SEGMENTS AS FEASIBLE

LONG TERM -
 • BIKE LANE - CROSS SECTION A OR
 • BUFFERED BIKE LANE - CROSS SECTION B

SHORT TERM -
 • MONITOR CTDOT VIP PAVING PROGRAM
 • MAXIMIZE SHOULDERS/NARROW VEHICLE LANES
 • PROVIDE BIKE LANES FOR LONG SEGMENTS AS FEASIBLE
 • CONTACT CTDOT ABOUT POSSIBLE STRATEGIC RADAR SPEED FEED BACK SIGN INSTALLATION

LONG TERM -
 • BUFFERED BIKE LANES - CROSS SECTION B OR
 • MULTI-USE PATH - CROSS SECTION C

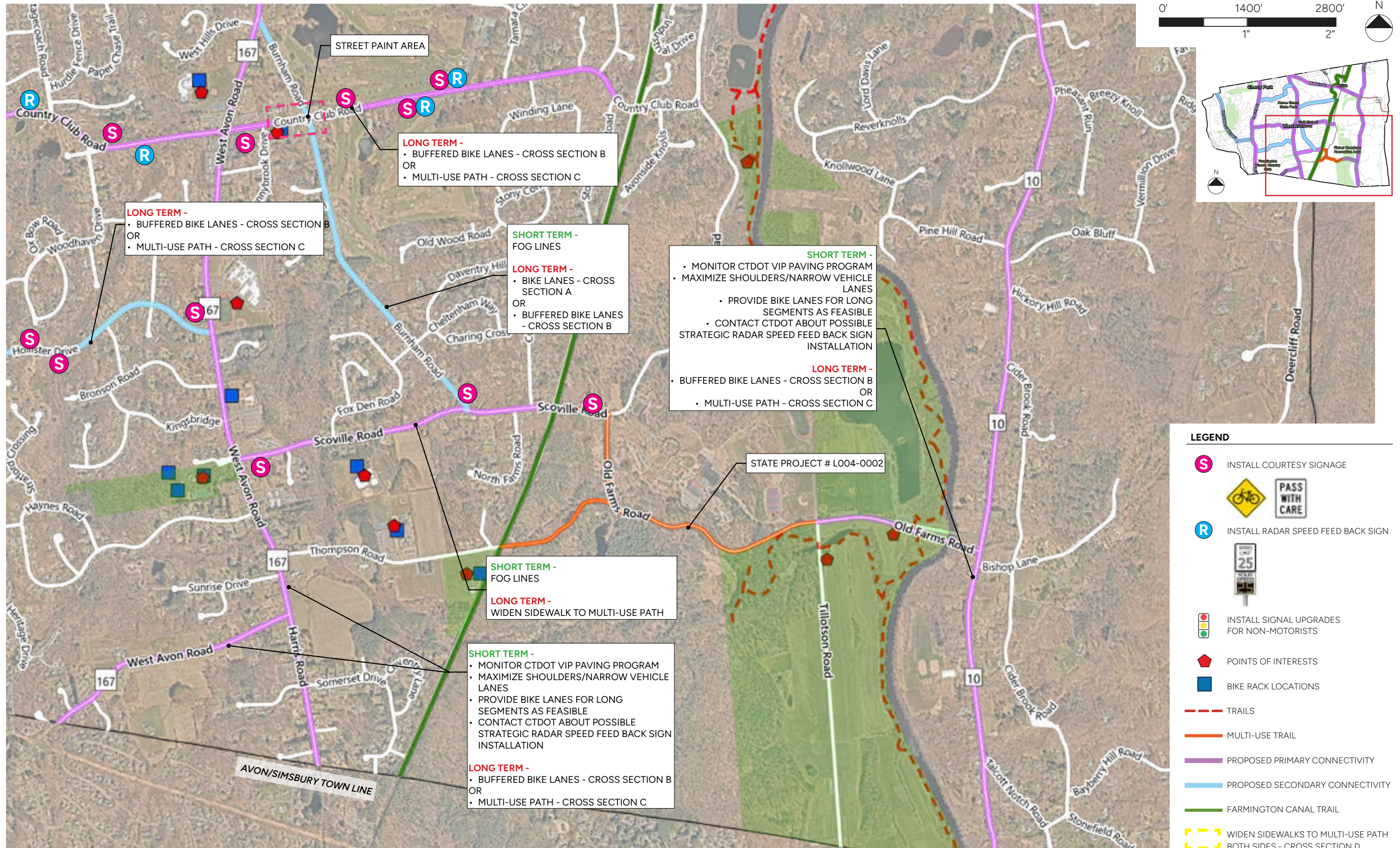
LONG TERM -
 • MAXIMIZE SHOULDERS/NARROW VEHICLE LANES
 • BIKE LANES - CROSS SECTION A

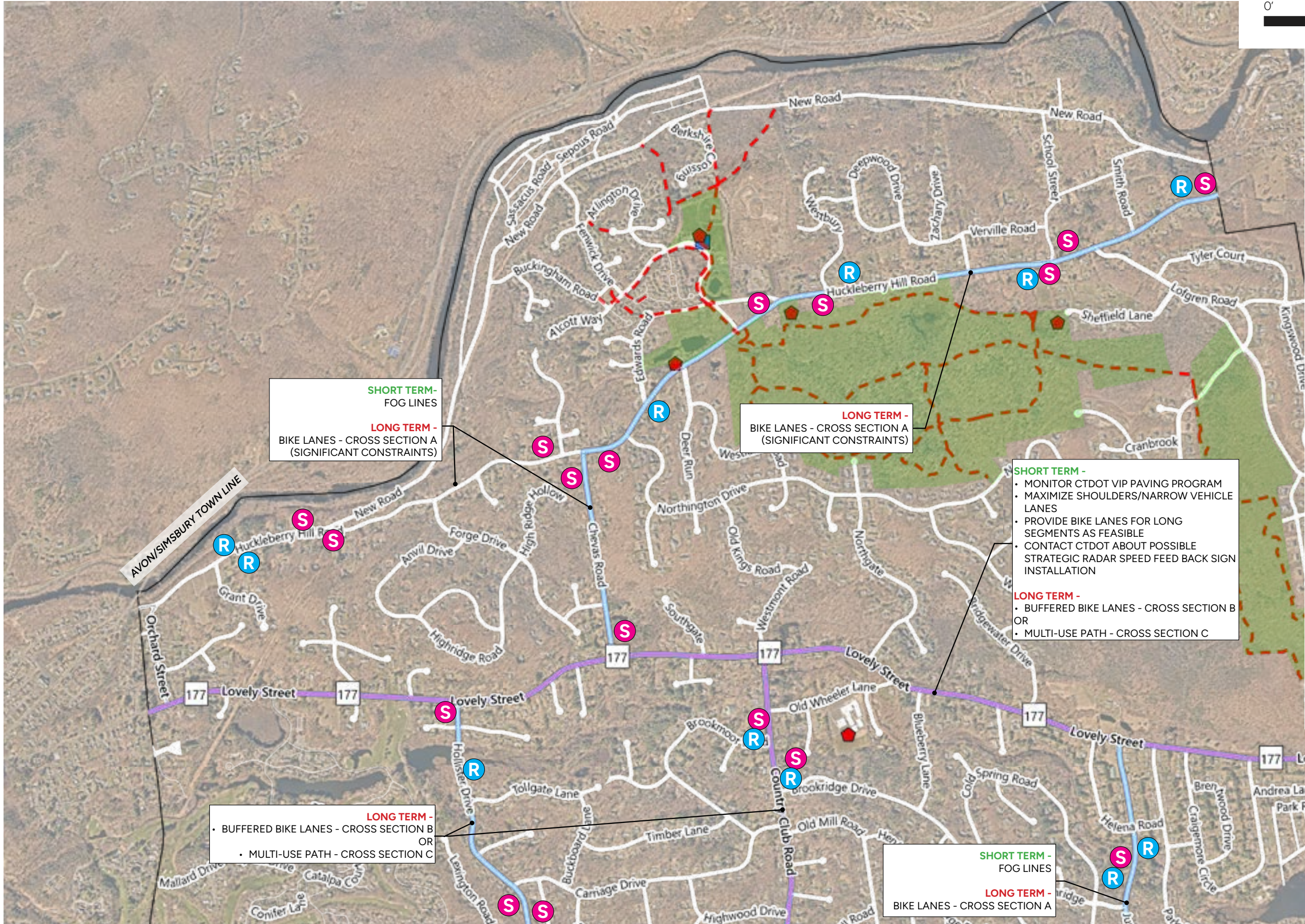
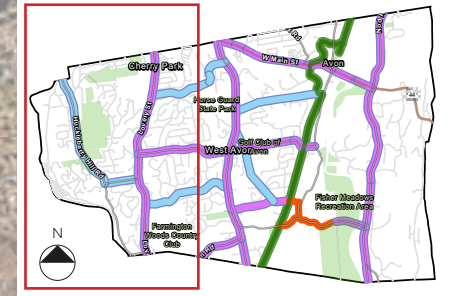
LONG TERM -
 REAR SERVICE ROAD

LONG TERM -
 • BUFFERED BIKE LANES - CROSS SECTION B
 • MULTI-USE PATH (2-LANE) - CROSS SECTION C

- LEGEND**
- INSTALL COURTESY SIGNAGE
 -
 - INSTALL RADAR SPEED FEED BACK SIGN
 -
 - INSTALL SIGNAL UPGRADES FOR NON-MOTORISTS INCLUDING PEDESTRIAN PRIORITY SIGNAGE AND BIKE USE SIGNAL TO CROSS
 -
 - POINTS OF INTERESTS
 - BIKE RACK LOCATIONS
 - TRAILS
 - MULTI-USE TRAIL
 - PROPOSED PRIMARY CONNECTIVITY
 - PROPOSED SECONDARY CONNECTIVITY
 - FARMINGTON CANAL TRAIL
 - WIDEN OF SIDEWALKS TO MULTI-USE PATH BOTH SIDES - CROSS SECTION D
 - PROPOSED MULTI-USE PATH 10'-12'







SHORT TERM -
FOG LINES

LONG TERM -
BIKE LANES - CROSS SECTION A
(SIGNIFICANT CONSTRAINTS)

LONG TERM -
BIKE LANES - CROSS SECTION A
(SIGNIFICANT CONSTRAINTS)

SHORT TERM -

- MONITOR CTDOT VIP PAVING PROGRAM
- MAXIMIZE SHOULDERS/NARROW VEHICLE LANES
- PROVIDE BIKE LANES FOR LONG SEGMENTS AS FEASIBLE
- CONTACT CTDOT ABOUT POSSIBLE STRATEGIC RADAR SPEED FEED BACK SIGN INSTALLATION

LONG TERM -

- BUFFERED BIKE LANES - CROSS SECTION B OR
- MULTI-USE PATH - CROSS SECTION C

LONG TERM -

- BUFFERED BIKE LANES - CROSS SECTION B OR
- MULTI-USE PATH - CROSS SECTION C

SHORT TERM -
FOG LINES

LONG TERM -
BIKE LANES - CROSS SECTION A

LEGEND

- S** INSTALL COURTESY SIGNAGE
- PASS WITH CARE**
- R** INSTALL RADAR SPEED FEED BACK SIGN
- 25
- INSTALL SIGNAL UPGRADES FOR NON-MOTORISTS
- POINTS OF INTERESTS
- BIKE RACK LOCATIONS
- TRAILS
- MULTI-USE TRAIL
- PROPOSED PRIMARY CONNECTIVITY
- PROPOSED SECONDARY CONNECTIVITY
- FARMINGTON CANAL TRAIL
- WIDEN SIDEWALKS TO MUTLI-USE PATH BOTH SIDES - CROSS SECTION D
- PROPOSED MULTI-USE PATH 10'-12'



Appendix D Implementation Matrix

Bicycle and Pedestrian Master Plan

Town of Avon

SLR Project No.: 141.13130.00016

February 29, 2024



Key: \$ = Less than \$100,000 | \$\$ = \$100,000 to \$300,000 | \$\$\$ = Greater than \$300,000

IMPLEMENTATION MATRIX
Avon Bicycle and Pedestrian Master Plan

Improvement	Figure/Section Reference	User (Bike/Ped/All)	Timeframe	Cost	Safety Priority?	Connectivity Priority?	Notes
Arch Road							
Install courtesy signage (bicycle image sign, PASS WITH CARE sign) at Farmington Canal Trail and Haber Drive	Bike Recommendations NE Section	B	SHORT - TERM	\$			
Install buffered bike lane as shown in cross-section B	Bike Recommendations NE Section	B	LONG - TERM	\$\$\$			Alternative: install multi-use path
Install multi-use path as shown in cross-section C	Bike Recommendations NE Section	A	LONG - TERM	\$\$\$			Alternative: install buffered bike lanes
Install new sidewalk between Security Drive and Old Farms Road	Walkshed Index Map Section 4	P	SHORT - TERM	\$		✓	
Install new crosswalk and ADA-compliant ramps at Security Drive	Walkshed Index Map Section 4	P	SHORT - TERM	\$		✓	
Chevas Road							
Install courtesy signage (bicycle image sign, PASS WITH CARE sign) near the intersections with Huckleberry Hill Road and Lovely Street	Bike Recommendations WEST Section	B	SHORT - TERM	\$			
Install fog lines	Bike Recommendations WEST Section	A	SHORT - TERM	\$	✓		
Install bike lanes as shown in cross-section A	Bike Recommendations WEST Section	B	LONG - TERM	\$\$\$			
Climax Heights Road							
Add additional Farmington Canal Trail wayfinding signage near Avon Town Hall	Walkshed Index Map Section 4	A	SHORT - TERM	\$		✓	
Install new sidewalk and install corner bumpout to shorten crossing distance near Avon Town Hall	Walkshed Index Map Section 4	P	SHORT - TERM	\$\$	✓	✓	
Country Club Road							
Install radar speed feedback signs near intersection with Stagecoach Road	Bike Recommendations NE Section	A	SHORT - TERM	\$	✓		
Install courtesy signage (bicycle image sign, PASS WITH CARE sign) between Burnham Road and Tamara Circle	Bike Recommendations SE Section	B	SHORT - TERM	\$			
Install courtesy signage (bicycle image sign, PASS WITH CARE sign) between Woodhaven Drive and West Avon Road, at at the intersection with West Avon Road	Bike Recommendations SE Section	B	SHORT - TERM	\$			
Install radar speed feedback signs between Woodhaven Drive and West Avon Road	Bike Recommendations SE Section	A	SHORT - TERM	\$	✓		
Install radar speed feedback signs between Burnham Road and Tamara Circle	Bike Recommendations SE Section	A	SHORT - TERM	\$	✓		
Install buffered bike lanes as shown in cross-section B	Bike Recommendations SE Section	B	LONG - TERM	\$\$			Alternative: install multi-use path
Install crosswalk, Rectangular Rapid Flashing Beacons (RRFBs) "pedestrian ahead" warning signage, and striping at Carriage Drive	Walkshed Index Map Section 14	P	SHORT - TERM	\$		✓	
Install new sidewalk from Lovely Street to Carriage Drive	Walkshed Index Map Section 14	P	SHORT - TERM	\$\$		✓	
Upgrade existing sidewalk from West Avon Road to Sunnybrook Drive (COMPLETED SUMMER 2023)	Walkshed Index Map Section 7	P	SHORT - TERM	\$			Already completed, summer 2023
Install new crosswalks and ADA-compliant ramps at Burnham Road, Winding Lane, Avonside and Stony Corners Road	Walkshed Index Map Section 7 , 7, and 8	P	SHORT - TERM	\$			
Install potential intersection art/treatment at Burnham Road, Sunnybrook Drive, and the Library	Walkshed Index Map Section 7 and Bike Recommendations SE Section	A	SHORT - TERM	\$\$			

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IMPLEMENTATION MATRIX
Avon Bicycle and Pedestrian Master Plan

Improvement	Figure/Section Reference	User (Bike/Ped/All)	Timeframe	Cost	Safety Priority?	Connectivity Priority?	Notes
Install pedestrian facility from Avon Public Library to Old Farms Road (potential multi-use path)	Walkshed Index Map Section 7, 7, and 8 and Bike Recommendations SE	A	LONG - TERM	\$\$\$		✓	Possible LOTCIP grant
Deercliff Road							
Create trail connection between Bishop Lane and Deercliff Road through existing utility easement	Walkshed Index Map Section 11	P	SHORT - TERM	\$\$		✓	
Hollister Drive							
Install courtesy signage (bicycle image sign, PASS WITH CARE sign) west of Crestwood Drive, and near intersection with West Avon Road	Bike Recommendations SE Section	B	SHORT - TERM	\$			
Install buffered bike lanes as shown in cross-section B	Bike Recommendations SE Section	B	LONG - TERM	\$\$\$			Alternative: install multi-use path
Install multi-use path as shown in cross-section C	Bike Recommendations SE Section	A	LONG - TERM	\$\$\$			Alternative: install buffered bike lanes
Huckleberry Hill Road							
Install courtesy signage (bicycle image sign, PASS WITH CARE sign) near the intersections with New Road, Chevas Road, Buckingham Road, Westbury Road, School Street, and Cemetery Road	Bike Recommendations WEST Section	B	SHORT - TERM	\$			
Install radar speed feedback signs near the intersections with Grant Drive, Edwards Road, Westbury Road, School Street, and Cemetery Road	Bike Recommendations WEST Section	A	SHORT - TERM	\$	✓		
Install bike lanes as shown in cross-section A	Bike Recommendations WEST Section	B	LONG - TERM	\$\$\$			Significant constraints
Install new sidewalk from Buckingham Road to Verville Road	Walkshed Index Map Section 12	P	LONG - TERM	\$\$			Sidewalk feasibility at this section is dependent on road shift
Shift road to avoid obstruction in right-of-way and reduce vehicular speeds north of Buckingham Road and north of Westbury Road	Walkshed Index Map Section 12	A	LONG - TERM	\$\$\$			
Formalize existing path and widen to 4-foot trail, replace guiderail south of Buckingham Road	Walkshed Index Map Section 12 and 13	P	LONG - TERM	\$\$			
Install new crosswalks and ADA-compliant ramps at Westbury Road, Buckingham Road, Northington Drive, and Chevas Road	Walkshed Index Map Section 12 and 13	P	SHORT - TERM	\$			
Install crosswalk, Rectangular Rapid Flashing Beacons (RRFBs) "pedestrian ahead" warning signage, and striping at Deer Run and Edwards Road	Walkshed Index Map Section 13	P	SHORT - TERM	\$			
Install new sidewalk from end of new trail to Countryside Park and from Edwards Road to south of Buttonwood Hill Drive	Walkshed Index Map Section 13	P	SHORT - TERM	\$		✓	
Juniper Drive							
Install fog lines	Bike Recommendations WEST Section	A	SHORT - TERM	\$	✓		
Install bike lanes as shown in cross-section A	Bike Recommendations WEST Section	B	LONG - TERM	\$\$\$			
Lovely Street (CT-177)							
Contact CTDOT about possible strategic radar speed feedback sign installation	Bike Recommendations WEST Section	A	SHORT - TERM	\$	✓		
Maximize shoulders/narrow vehicle lanes	Bike Recommendations WEST Section	A	SHORT - TERM	\$	✓		Monitor CTDOT VIP Paving Program
Provide bike lanes for long segments as feasible	Bike Recommendations WEST Section	B	SHORT - TERM	\$\$\$			
Install buffered bike lanes as shown in cross-section B	Bike Recommendations WEST Section	B	LONG - TERM	\$\$\$			Alternative: install multi-use path

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IMPLEMENTATION MATRIX
Avon Bicycle and Pedestrian Master Plan

Improvement	Figure/Section Reference	User (Bike/Ped/All)	Timeframe	Cost	Safety Priority?	Connectivity Priority?	Notes
Install multi-use path as shown in cross-section C	Bike Recommendations WEST Section	A	LONG - TERM	\$\$\$			Alternative: install buffered bike lanes and sidewalk
Install crosswalk, Rectangular Rapid Flashing Beacons (RRFBs) "pedestrian ahead" warning signage, and striping at Northgate	Walkshed Index Map Section 14	P	SHORT - TERM	\$			
Install new sidewalk from Blueberry Lane to Northgate	Walkshed Index Map Section 14	P	SHORT - TERM	\$			Alternative: install multi-use path
Mountain View Avenue							
Install multi-use path from Route 44 to existing sidewalk near Manion Lane	Bike Recommendations NE Section	A	LONG - TERM	\$\$			
Install new sidewalk from Route 44 to existing sidewalk near Manion Lane	Walkshed Index Map Section 2 and 3	P	SHORT - TERM	\$		✓	Alternative: upgrade to multi-use path
Nod Road							
Install courtesy signage (bicycle image sign, PASS WITH CARE sign) north and south of Woodford Hills Drive	Bike Recommendations NE Section	B	SHORT - TERM	\$			
Install radar speed feedback signs near intersections with Woodford Hills Drive and Nod Way	Bike Recommendations NE Section	A	SHORT - TERM	\$	✓		
Maximize shoulders/narrow vehicle lanes through town repaving/maintenance	Bike Recommendations NE Section	A	SHORT - TERM	\$	✓		
Install bike lane as shown in cross-section A	Bike Recommendations NE Section	B	LONG - TERM	\$\$\$			Alternative: install buffered bike lanes
Install buffered bike lane as shown in cross-section B	Bike Recommendations NE Section	B	LONG - TERM	\$\$\$			Alternative: install bike lanes (no buffer)
Extend culvert on Nod Road to accommodate new sidewalk	Walkshed Index Map Section 1	P	LONG - TERM	\$\$			
Install new sidewalk from Route 44 to Hunters Run	Walkshed Index Map Section 1	P	SHORT - TERM	\$			Extend culvert over brook near The North House
Old Farms Road							
Install crosswalk, Rectangular Rapid Flashing Beacons (RRFBs) "pedestrian ahead" warning signage, and striping at Country Club Road	Walkshed Index Map Section 8	P	SHORT - TERM	\$		✓	
Install new sidewalk south of Country Club Road, connecting to Fisher Meadows	Walkshed Index Map Section 8	P	SHORT - TERM	\$		✓	
Extend sidewalk from seating area near Waterville Road to new crosswalk across Waterville Road (CT-10)	Walkshed Index Map Section 11	P	SHORT - TERM	\$		✓	
Old Wheeler Lane							
Install new crosswalks and ADA-compliant ramps at Lovely Street, Old Wheeler Court, Roaring Brook School exit, Camelot Lane, and Country Club Road	Walkshed Index Map Section 14	P	SHORT - TERM	\$			
Install new sidewalk from Lovely Street to Country Club Road	Walkshed Index Map Section 14	P	SHORT - TERM	\$			
Route 44 (West/East Main Street)							
Add sidewalk signage showing pedestrian priority strategically located throughout the Route 44 corridor	Bike Recommendations NE Section	A	SHORT - TERM	\$	✓		
Create rear service road from Plaza 44 to Dale Road	Bike Recommendations NE Section	A	LONG - TERM	\$\$\$			Alternative: install multi-use path
Install multi-use path as shown in cross-section D on north and south side from Nod Road to Ensign Drive	Bike Recommendations NE Section	A	LONG - TERM	\$\$\$			

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IMPLEMENTATION MATRIX
Avon Bicycle and Pedestrian Master Plan

Improvement	Figure/Section Reference	User (Bike/Ped/All)	Timeframe	Cost	Safety Priority?	Connectivity Priority?	Notes
Install multi-use path from Ensign Drive to Plaza 44	Bike Recommendations NE Section	A	LONG - TERM	\$\$\$			
Install or widen sidewalk to multi-use path between the Canton town line and Plaza 44	Bike Recommendations NE Section	A	LONG - TERM	\$\$\$			Alternative: create service road
Install new crosswalks and ADA-compliant ramps at Nod Road/Waterville Road (Route 10)	Walkshed Index Map Section 1	P	SHORT - TERM	\$			
Upgrade pedestrian signals at Nod Road	Walkshed Index Map Section 1	P	LONG - TERM	\$\$	✓		
Restripe (narrow) travel lanes and shoulder lines, and install protective barriers and sidewalk to create pedestrian walkway on bridge over Farmington River	Walkshed Index Map Section 1 and 2	A	SHORT - TERM	\$\$			
Extend existing trail in River Park and add wayfinding signage	Walkshed Index Map Section 2	P	SHORT - TERM	\$			
Install new crosswalks and ADA-compliant ramps at Old Avon Village east driveway (51 East Main Street) and at Mountain View Avenue	Walkshed Index Map Section 2	P	SHORT - TERM	\$			
Install new sidewalk on north side of Route 44 from 56-70 East Main Street (Route 44) to Farmington River bridge	Walkshed Index Map Section 2	P	SHORT - TERM	\$			
Install new sidewalk on south side of Route 44 between River Park and Farmington River bridge	Walkshed Index Map Section 2	P	SHORT - TERM	\$			
Upgrade pedestrian signals at Old Avon Village east driveway (51 East Main Street)	Walkshed Index Map Section 2	P	LONG - TERM	\$\$	✓		
Construct retaining wall and install new sidewalk near Avon Village from existing sidewalk on Towpath Lane to 51 East Main Street access road	Walkshed Index Map Section 2 and 3	P	LONG - TERM	\$\$\$			
Install ADA-compliant landing area at existing bus stops	Walkshed Index Map Section 2, 3, and 6	P	SHORT - TERM	\$			
Install new crosswalks and ADA-compliant ramps at Avon Village W Intersection	Walkshed Index Map Section 3	P	SHORT - TERM	\$			
Upgrade pedestrian signals at Avon Village West Intersection	Walkshed Index Map Section 3	P	LONG - TERM	\$\$	✓		
Install new crosswalks and ADA-compliant ramps at Ensign Drive	Walkshed Index Map Section 4	P	SHORT - TERM	\$			
Upgrade pedestrian signals at Ensign Drive	Walkshed Index Map Section 4	P	LONG - TERM	\$\$	✓		
Install new sidewalk between Ensign Dr. and Darling Dr., continuing south on Darling Dr. to connect to Farmington Canal Trail	Walkshed Index Map Section 4 and 5	P	SHORT - TERM	\$\$		✓	
Install new crosswalks and ADA-compliant ramps between River Ridge and Lawrence Ave	Walkshed Index Map Section 5	P	SHORT - TERM	\$			
Install new sidewalk in front of Whole Foods and construct stair connection to Farmington Canal Trail or ADA-grade path as feasible	Walkshed Index Map Section 5	P	LONG - TERM	\$\$			
Install new sidewalk from Whole Foods to Lawrence Ave.	Walkshed Index Map Section 5 and 6	P	SHORT - TERM	\$\$			
Construct pedestrian bridge across Nod Brook	Walkshed Index Map Section 6	P	LONG - TERM	\$\$			
Install new crosswalks and ADA-compliant ramps near Plaza 44, Walmart, and Shops at Nod Brook	Walkshed Index Map Section 6	P	SHORT - TERM	\$			
Install new sidewalk between Plaza 44 and the Shops at Nod Brook	Walkshed Index Map Section 6	P	LONG - TERM	\$\$			

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IMPLEMENTATION MATRIX
Avon Bicycle and Pedestrian Master Plan

Improvement	Figure/Section Reference	User (Bike/Ped/All)	Timeframe	Cost	Safety Priority?	Connectivity Priority?	Notes
Upgrade pedestrian signals at Plaza 44 driveway, Walmart access road, and Shops at Nod Brook access road	Walkshed Index Map Section 6	P	LONG - TERM	\$\$	✓		
Scoville Road							
Install courtesy signage (bicycle image sign, PASS WITH CARE sign) at West Avon Road, Burnham Road, and Old Farms Road	Bike Recommendations SE Section	B	SHORT - TERM	\$			
Install fog lines	Bike Recommendations SE Section	A	SHORT - TERM	\$	✓		
Widen existing sidewalk to multi-use path from West Avon Road to the Farmington Canal Trail	Walkshed Index Map Section 10 and 11 and Bike Recommendations SE Section	A	LONG - TERM	\$\$\$			
Simsbury Road (U.S. Route 202/CT-10)							
Install pedestrian island and add Rectangular Rapid Flashing Beacon (RRFB) to minimize multiple threat at existing crosswalk	Walkshed Index Map Section 3	P	LONG - TERM	\$\$	✓		
Thompson Road							
Construct pedestrian bridge across Thompson Brook	Walkshed Index Map Section 10	P	LONG - TERM	\$\$			
Install new crosswalks and ADA-compliant ramps at Thompson Brook School	Walkshed Index Map Section 10	P	SHORT - TERM	\$			
Install new sidewalk from West Avon Road to Farmington Canal Trail	Walkshed Index Map Section 10 and 11	P	LONG - TERM	\$\$			
Waterville Road (CT-10)							
Install multi-use path as shown in cross-section C	Bike Recommendations NE and SE Section	A	LONG - TERM	\$\$\$			
Contact CTDOT about possible strategic radar speed feedback sign installation	Bike Recommendations NE and SE Sections	A	SHORT - TERM	\$	✓		
Maximize shoulders/narrow vehicle lanes through repaving/maintenance	Bike Recommendations NE Section	A	SHORT - TERM	\$	✓		Monitor CTDOT VIP Paving Program
Provide bike lanes for long segments as feasible	Bike Recommendations NE Section	B	SHORT - TERM	\$\$\$			
Install buffered bike lane as shown in cross-section B	Bike Recommendations NE and SE Section	B	LONG - TERM	\$\$\$			
Install new sidewalk from Route 44 to existing trail system at Alsop Meadows	Walkshed Index Map Section 1	P	SHORT - TERM	\$			
Install new crosswalk and ADA-compliant ramps at Old Farms Road	Walkshed Index Map Section 11	P	SHORT - TERM	\$		✓	
Install new sidewalk between Old Farms Road and Bishop Lane	Walkshed Index Map Section 11	P	SHORT - TERM	\$		✓	
West Avon Road (CT-167)							
Install courtesy signage (bicycle image sign, PASS WITH CARE sign) at Woodmont Road, Arch Road	Bike Recommendations NE Section	B	SHORT - TERM	\$			
Contact CTDOT about possible strategic radar speed feedback sign installation	Bike Recommendations SE Section	A	SHORT - TERM	\$	✓		
Maximize shoulders/narrow vehicle lanes through repaving/maintenance	Bike Recommendations SE Section	A	SHORT - TERM	\$	✓		Monitor CTDOT VIP Paving Program
Provide bike lanes for long segments as feasible	Bike Recommendations SE Section	B	SHORT - TERM	\$\$\$			
Install buffered bike lanes as shown in cross-section B	Bike Recommendations SE Section	B	LONG - TERM	\$\$\$			Alternative: install multi-use path

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IMPLEMENTATION MATRIX
Avon Bicycle and Pedestrian Master Plan

Improvement	Figure/Section Reference	User (Bike/Ped/All)	Timeframe	Cost	Safety Priority?	Connectivity Priority?	Notes
Install multi-use path as shown in cross-section C	Bike Recommendations SE Section	A	LONG - TERM	\$\$\$			Alternative: install buffered bike lanes and sidewalk
Install new sidewalk from Scoville Road to Thompson Road	Walkshed Index Map Section 10	P	SHORT - TERM	\$\$			Alternative: install multi-use path
Upgrade pedestrian signals at Scoville Road	Walkshed Index Map Section 10	P	LONG - TERM	\$\$	✓		
Upgrade existing sidewalk from Avon Middle School to Scoville Road	Walkshed Index Map Section 7, 9, and 10	P	SHORT - TERM	\$\$			
Install new crosswalks and ADA-compliant ramps between Country Club Road and Scoville Road	Walkshed Index Map Section 9 and 10	P	SHORT - TERM	\$			
Woodmont Road							
Install courtesy signage (bicycle image sign, PASS WITH CARE sign) at Stagecoach Road	Bike Recommendations NE Section	B	SHORT - TERM	\$			
Install radar speed feedback signs near intersection with Stagecoach Road	Bike Recommendations NE Section	A	SHORT - TERM	\$	✓		

